



# Rome-Floyd County MPO Bicycle, Pedestrian, and Trail Master Plan

2015

Prepared For:  
Rome-Floyd County MPO



Prepared By:





***RESOLUTION Adopting 2015 Rome-Floyd County Metropolitan  
Planning Organization***

***Bicycle, Pedestrian, and Trail Master Plan***

**Whereas**, the Rome-Floyd County Metropolitan Planning Organization (MPO) prepares a Long Range Transportation Plan (LRTP) every four years using the applicable Federal Standards, in this case, **Moving Ahead for Progress in the 21st Century (MAP-21)** legislation, signed into law in 2012 ; and

**Whereas**, the LRTP is required to incorporate an intermodal approach to transportation planning; and

**Whereas**, the citizens of the Rome-Floyd County Metropolitan Planning Area attended bike/ped planning workshops and identified the need to provide safe, efficient, and well-connected facilities for the use of bicyclists and pedestrians; and

**Whereas**, the priorities set by citizens of the Rome-Floyd Metropolitan Planning Area were incorporated into the 2015 Rome-Floyd County MPO Bicycle, Pedestrian, and Trail Master Plan; and

**Whereas**, a public comment period seeking public input to the Rome-Floyd County MPO Bicycle, Pedestrian, and Trail Master Plan was met according to the 2014 Public Participation Plan;

**NOW, therefore**, the Transportation Policy Committee (TPC) of the Rome-Floyd County Metropolitan Planning Organization (MPO) does hereby approve and adopt the 2015 Rome-Floyd County MPO Bicycle, Pedestrian, and Trail Master Plan).

  
\_\_\_\_\_  
Irwin Bagwell, Commissioner and  
Chair, Transportation Policy Committee

MAY 28, 2015  
Date

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# Bicycle, Pedestrian, and Trail Master Plan



## Purpose

Leveraging the benefits that accrue to communities where people have the option to walk and bike requires intentional planning. The *Rome-Floyd County MPO Bicycle, Pedestrian, Trail Master Plan* is the result of a comprehensive look at strategies for improving the safety, convenience, and enjoyment of walking and bicycling in Rome-Floyd County.

## Community Priorities

During the community workshop, participants were asked to prioritize the types of programs and infrastructure they would like to see implemented. Below is a summary of the top three priorities for each category.

### Infrastructure

*What would you most like to see in Rome-Floyd County?*

- Trails and Greenways
- Bicycle Lanes/Buffered Bike Lanes
- Shared-Use Side Paths Along Roadways



Trails and greenways are the top infrastructure priority.

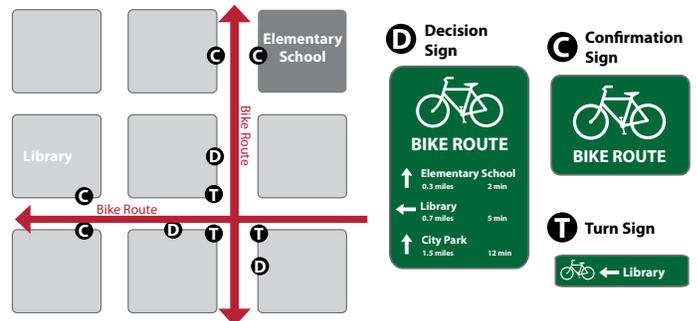
## Goals

- Achieve Bicycle Friendly Community Designation by 2018
- Achieve Walk Friendly Community Designation by 2018
- Double the number of people that walk and bike to work within 5 years
- Create a 27 mile on-street bikeway network
- Create a 45 mile trail network
- Expand the sidewalk network along major roadways and around parks and schools

## Programs

*What new programs (or expansion of existing programs) would have the greatest impact on walking and bicycling in Rome-Floyd County?*

- Wayfinding Signage
- Trail User Etiquette Campaign
- Bicycle and Pedestrian Safety Campaign



Wayfinding signage is the top programs priority.

## Projects

The plan outlines a connected network of sidewalks, bikeways, and trails that serve everyday destinations including schools, parks, employment centers, and neighborhoods.



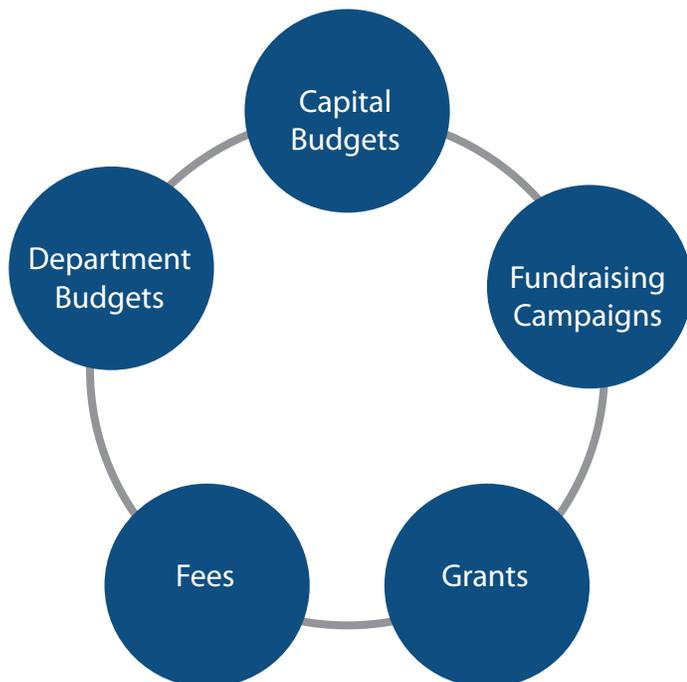
## Programs

The plan describes a set of education, encouragement, enforcement and evaluation programs that complement recommended walking and biking infrastructure investments. Program ideas include fun and educational classes for children, open streets events, and targeted enforcement.



## Policy and Funding

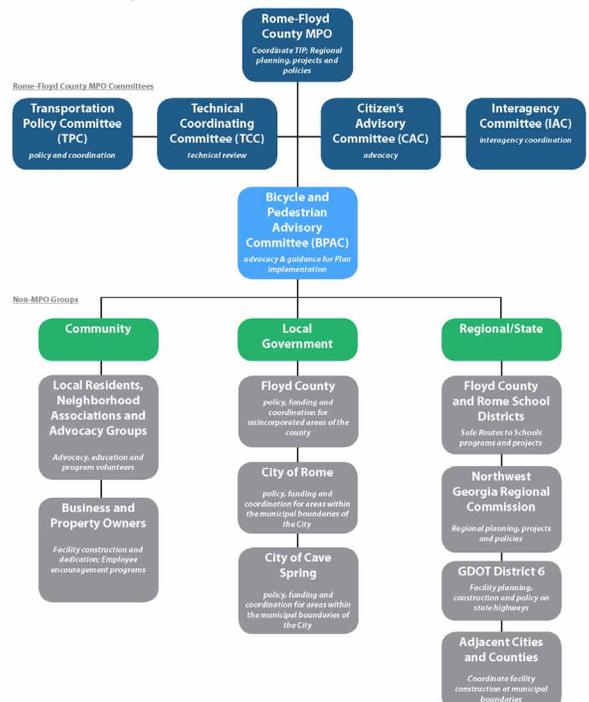
The plan identifies policies and funding recommendations that support timely implementation, including guidance on drafting a Complete Streets policy, forming a bicycle and pedestrian advisory committee, and suggestions for pursuing funds.



## Implementation

The plan includes a detailed implementation section that outlines how multiple stakeholders can work together to realize the vision created during the planning process.

### Roles for Implementation



**CHAPTER OUTLINE:****Overview****Vision****Key Goals****Goals****Planning Process****Community Priorities**

## Overview

In the summer of 2014, the Rome-Floyd County MPO began developing a bicycle, pedestrian, and trail plan for Rome-Floyd County. The purpose of the Plan is to provide a clear framework for the development of new facilities, programs, and policies that will support safe and convenient walking and biking conditions for transportation and recreation.

The plan is organized into the following sections:

### Introduction

Provides an overview of the project as well as the vision, goals, and summary of the public planning process.

### Needs Analysis

Documents existing conditions and analysis.

### Recommendations

Presents recommendations for bicycles, pedestrians, trails, support facilities, programs, and policies.

### Implementation

Summarizes the responsibilities, action steps, and strategies to achieve the vision and goals established by this Plan.

## Vision



“Walking and biking in Rome-Floyd County will be **safe, convenient, and enjoyable forms of transportation and recreation**. Creating favorable walking and biking conditions and expanding route options for a wider range of cyclists and pedestrians is **key for the community’s economic, environmental, social, and physical well-being**. When implemented, the improved and connected walkway and bikeway network will **enhance connections between neighborhoods, job centers, tourist attractions, shops, schools, places of worship, and other daily destinations.**”

## **Key Goals**

When this Plan is fully implemented, the following key policy and infrastructure accomplishments will be achieved:

**Achieve Bicycle Friendly Community designation by 2018**

**Achieve Walk Friendly Community designation by 2018**

**Double the number of people commuting by walking within 5 years**

**Double the number of people commuting by bike within 5 years**

**Create 27 mile on-street bikeway network**  
(2 miles currently; 25 miles proposed)

**Create 45 mile trail network**  
(10 miles currently; 35 miles proposed)

## Goals

The goals for this plan are organized based on the “Five E’s” for a Bicycle Friendly or Walk Friendly Community: Engineering, Education, Encouragement, Enforcement, and Evaluation/Planning as well as a sixth “E” of Equity. The Bicycle Friendly Community (BFC) and Walk Friendly Community (WFC) programs are national initiatives intended to encourage cities and towns across the country to improve the walking and biking environment in their community and to recognize communities who are successfully doing this. The goals and recommendations for this Plan are structured to position Rome-Floyd County to achieve both designations.

## Engineering

- Create a connected network of sidewalks and pedestrian pathways
- Create a connected network of on-street bikeways and off-street pathways
- Connect all public schools to the trail system
- Connect all of the universities, colleges, and Downtown Rome by trail
- Connect hospitals, the county health department, and major medical facilities to the trail system
- Connect major employment centers to the trail system
- Connect apartment developments, neighborhoods, and other major residential areas to the trail system
- Create a coherent, recognizable signage system
- Connect Cave Spring to Rome with a trail
- Create additional trailhead locations for access, parking, restrooms, and information



## Education

- Establish a regular bicycle and walking education program for area elementary schools
- Create a family biking class
- Continue to develop heritage and ecological education elements with trails

## Encouragement

- Host one open street event per year
- Develop a Bike Month activities program

## Enforcement

- Create a bicycle and pedestrian education and enforcement program with law enforcement partners

## Equity

- Provide equal access to the bikeway and walkway network for all ages, abilities, and backgrounds through public engagement, program delivery, and capital investment

## Evaluation/Planning

- Establish a bicycle and pedestrian committee
- Maintain an inventory of existing trails, sidewalks, and on-street bike facilities
- Adopt a complete streets policy
- Create dedicated funding or set aside for bicycle and pedestrian projects and programs
- Become a Bicycle Friendly Community by 2018
- Become a Walk Friendly Community by 2018
- Double the number of people commuting by walking within 5 years
- Double the number of people commuting by bike within 5 years



## Planning Process

*Kickoff Meeting – A project kickoff meeting with community members was held on August 24, 2014.*



*Workshops – Public meetings were held at the Forum on August 4 and 5, 2014. Meetings included an evening workshop on August 4 and open “office hours” for the community to stop by at their leisure on August 5.*



*Open House – A draft master plan presentation was given on October 21, 2014 at the Forum.*



*Final Presentation – A final draft presentation was given on December 16, 2014 at the Forum.*



**Public workshop participants voted on a series of infrastructure improvements and programs. The top priority program is wayfinding signage and the top infrastructure priorities are trails and greenways.**

## Community Priorities

At the public workshops held over two days on August 4 and 5, 2014, community members were asked to prioritize different types of infrastructure improvements and programs. The following sections summarize the results of the voting exercise. The results were used to develop the recommendations for this Plan.

### Programs

What new programs (or expansion of existing programs) would have the greatest impact on walking and bicycling in Rome-Floyd County?

## EDUCATION, ENCOURAGEMENT, AND ENFORCEMENT

Rome-Floyd County MPO  
Bicycle and Pedestrian Plan



### FOR BIKING AND WALKING

An inviting network of pedestrian and bicycle facilities for all ages and abilities, providing access to destinations

Community understanding and respect for the sharing the road with ALL road users (bike, pedestrian, car, and truck)

Increased walking and biking activity



A safer environment for every road user

Institutional support and long-term planning for active transportation

A diverse population geographically, socioeconomically, and demographically

#### WHAT SUCCESSFUL PROGRAMS ALREADY EXIST IN ROME-FLOYD COUNTY?

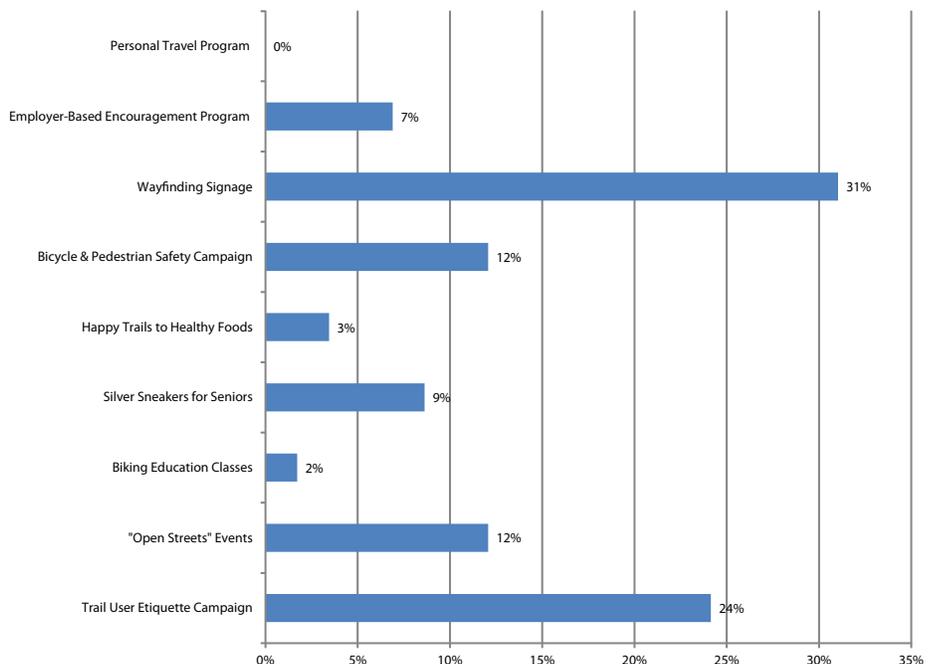


#### WHAT NEW PROGRAMS (OR EXPANSION OF EXISTING PROGRAMS) WOULD HAVE THE GREATEST IMPACT ON WALKING AND BICYCLING IN ROME-FLOYD COUNTY? (Spend your dots! You have 3 dots to vote for programs you think would provide the greatest benefit to the community.)

<p><b>TRAIL USER ETIQUETTE CAMPAIGN</b> A campaign for shared-use path and trail etiquette works to ensure that all users safely and responsibly share the trail. The campaign may include media, signage, brochures, volunteer "ambassadors", etc.</p>	<p><b>'OPEN STREETS' EVENTS</b> These events temporarily close a street to cars, but open them to create a temporary park that is available to the public for walking, bicycling, dancing, skateboarding, and more.</p>	<p><b>BIKING EDUCATION CLASSES</b> Bicycle Education Programs offer a range of bicycling education classes. They help bicyclists learn things such as how to commute to work or how to teach their children safe bicycling skills.</p>
<p><b>SILVER SNEAKERS FOR SENIORS</b> Interested agencies, nonprofits, health departments, and senior centers can partner to develop an active lifestyles program for senior citizens utilizing the bicycle, pedestrian, and greenways network. Activities could include adult bicycle or bicycle rides, nature walks, walks to lunch, and safety education.</p>	<p><b>HAPPY TRAILS TO HEALTHY FOODS</b> The important link between physical activity and healthy eating can be highlighted in a fun and interactive manner through promoting healthy food outlets along the bicycle, pedestrian, and greenway network.</p>	<p><b>BIKING &amp; PEDESTRIAN SAFETY CAMPAIGN</b> A high-profile marketing campaign is an effective strategy for highlighting the importance of respect and shared responsibility on the road between bicyclists, motorists, and pedestrians.</p>
<p><b>WAYFINDING SIGNAGE</b> Wayfinding signs direct users along the trail, bikeway, and walkway network and link users to community destinations. These signs can also include mileage, estimated travel time, and even calories burned.</p>	<p><b>EMPLOYER-BASED ENCOURAGEMENT PROGRAM</b> Companies that establish a bicycling and walking incentive program for employees increase the number of employees who commute to work, meetings, or lunch by biking or walking.</p>	<p><b>PERSONAL TRAVEL PROGRAM</b> PT programs are proven to reduce drive-alone trips by approximately 10% and increase bicycling and walking within a target area by providing customized route-planning resources, support, and encouragement materials.</p>

ARE THERE OTHER PROGRAM IDEAS YOU WOULD LIKE TO SEE IN THE REGION? (Spend your thoughts for ideas not included on existing ideas to show your support)

Figure 1. Program Voting Results



# Infrastructure

What would you most like to see in Rome-Floyd County?

PEDESTRIAN AND BICYCLE INFRASTRUCTURE

**Rome-Floyd County MPO**  
**Bicycle and Pedestrian Plan**

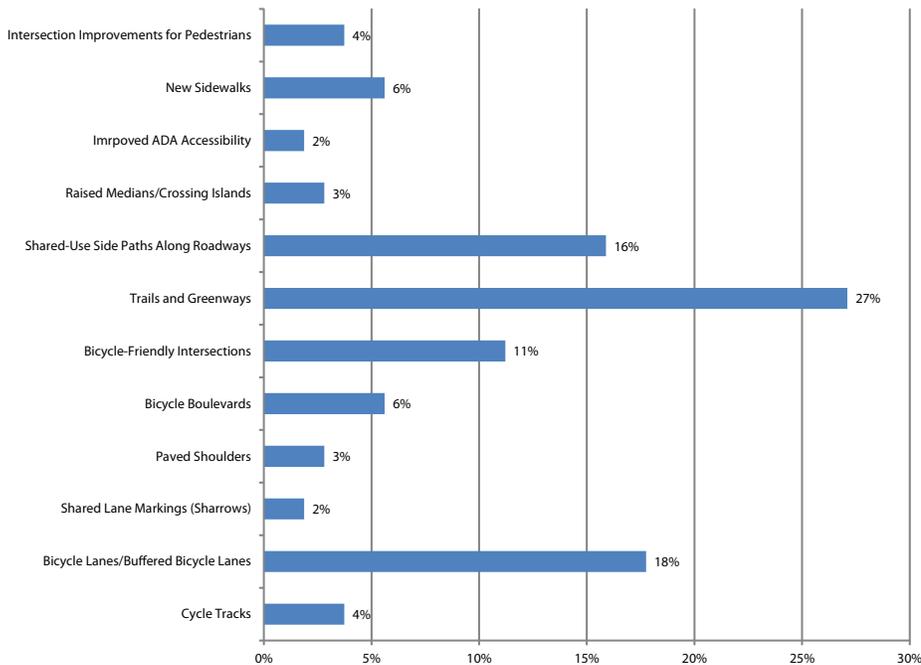
A Project of the Rome-Floyd County, Georgia Metropolitan Planning Organization



**What would you most like to see in Rome-Floyd County? (vote with 6 stickers)**

<p><b>CYCLE TRACKS</b></p>  <p><small>A cycle track is an exclusive bicycle facility physically separated from traffic and distinct from the sidewalk.</small></p>		<p><b>TRAILS AND GREENWAYS</b></p> 	
<p><b>BICYCLE LANES/BUFFERED BICYCLE LANES</b></p> 		<p><b>SHARED-USE SIDE PATHS ALONG ROADWAYS</b></p> 	
<p><b>SHARED LANE MARKINGS (SHARROWS)</b></p> 		<p><b>RAISED MEDIANS/CROSSING ISLANDS</b></p> 	
<p><b>PAVED SHOULDERS</b></p> 		<p><b>IMPROVED ADA ACCESSIBILITY</b></p> 	
<p><b>BICYCLE BOULEVARDS</b></p>  <p><small>Bicycle boulevards are streets with low traffic volumes and speeds, designated and designed to give priority to bicycle travel.</small></p>		<p><b>NEW SIDEWALKS</b></p> 	
<p><b>BICYCLE-FRIENDLY INTERSECTIONS</b></p> 		<p><b>INTERSECTION IMPROVEMENTS FOR PEDESTRIANS</b></p> 	

Figure 2. Infrastructure Voting Results





## CHAPTER OUTLINE:

Overview

Community Profile

Walking and Biking  
Trends

Existing Facilities

Community Input

Bicycle Friendly  
and Walk Friendly  
Community  
Assessment

Previous Plans and  
Policy

## Overview

Many factors influence the quality and quantity of walking and biking in a particular place. The presence of sidewalks and bike lanes, proximity to destinations, connectivity of the road network, and the number of people walking and biking are just a few. Further, these factors and others influence the viability of particular improvements and therefore will guide the recommendations of this Plan. Recommendations will vary throughout Floyd County given the different needs in rural and urban areas.

This chapter documents existing conditions and trends in Rome-Floyd County related to walking and biking. It also provides an overview of the Bicycle Friendly Community and Walking Friendly Community programs and how ready Rome-Floyd County is for key criteria for each program. Finally, previous planning efforts that relate to this Plan are summarized along with input from the public meetings for this Plan.

## Community Profile

Floyd County is located in Georgia and had a 2012 population of approximately 96,000. The county seat of Floyd County is Rome, which had a 2012 population of approximately 36,000, or roughly one third of Floyd County's residents. In addition, the City of Cave Spring is located southwest of Rome in Floyd County and was home to approximately 1,200 people as of the 2010 U.S. Census.

Based on the 2010 census, the population density in Rome is over six times greater than in Floyd County overall and almost seven times more than in Georgia as a whole. In addition, the population density in Cave Spring is almost two times greater than in Floyd County overall and over two times greater than Georgia overall. Table 1 summarizes the land areas per the 2010 census and population densities.

# Study Area: Rome and Floyd County

Rome-Floyd County, GA MPO Bicycle and Pedestrian Plan

-  Park
-  Water Feature
-  School
-  City Limits
-  Floyd County Limits

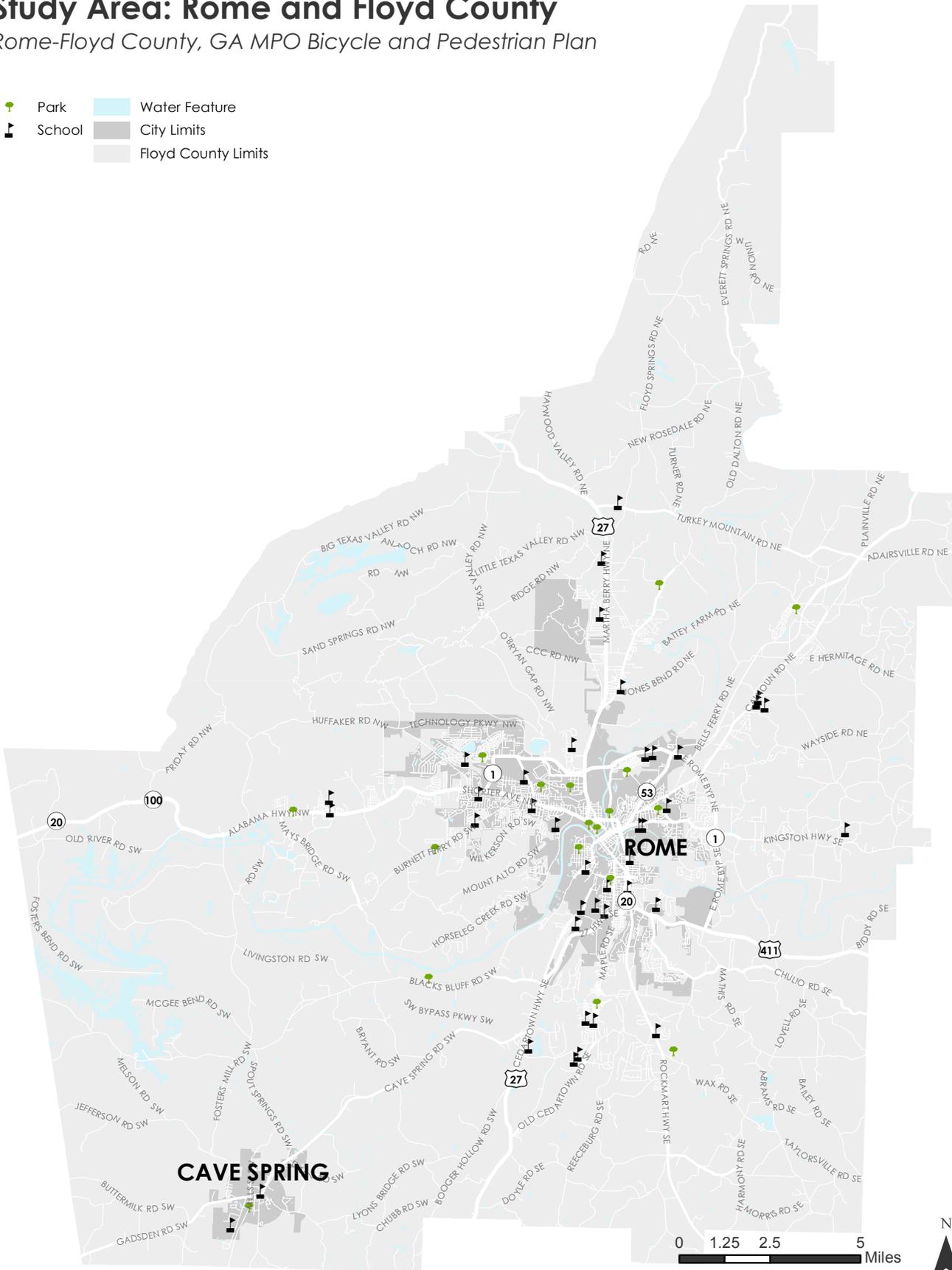


Table 1. Land Area, Population and Population Densities

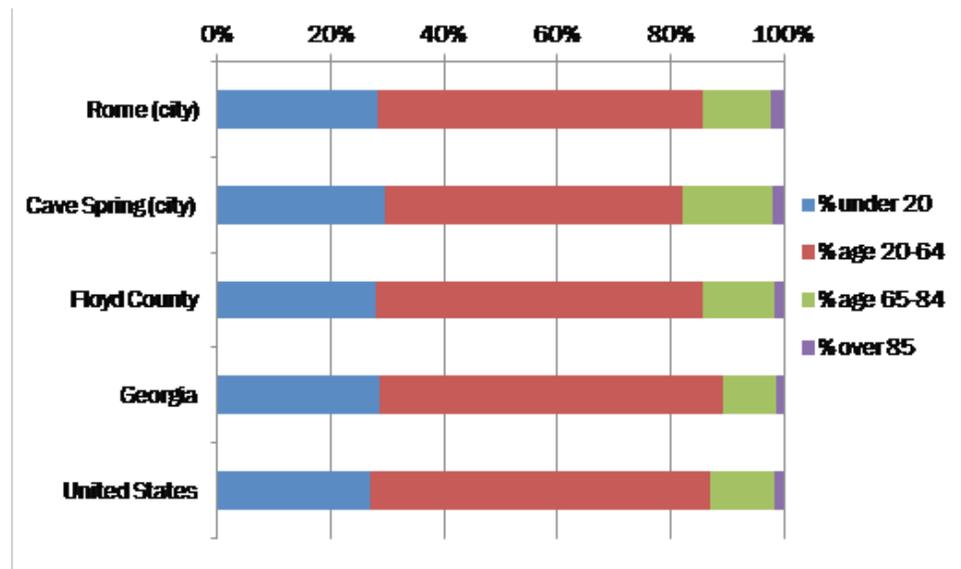
Community	Land Area (sq miles)	2012 Population	2010 Population Density (persons/sq mile)	2012 Population Density* (persons/sq mile)
Rome	30.91	36,159	1,174.4	1,169.8
Cave Spring	3.4	1,200**	352.4	352.4**
Floyd County	509.9	95,995	188.9	188.3
Georgia	57,513.5	9,915,646	168.4	172.41
U.S.	3,531,905.4	313,873,685	87.4	88.9

\*Assumed land areas remained consistent with the 2010 census and calculated density based on 2012 population estimates.

\*\*A 2012 population estimate is not available for the City of Cave Spring, so all populations presented are per the 2010 U.S. Census.

As of the 2010 census, 2.4% of Rome’s population was 85 or older, which is a larger percentage than in Floyd County (1.8%), Cave Spring (2.1%), Georgia (1.2%), and the U.S. (1.8%). Additionally, approximately 28% of Rome’s and Floyd County’s populations are under the age of 20 compared to around 29.4% in Cave Spring and 28.7% in Georgia overall.

Figure 3. Population Age Distribution



## Walking and Biking Trends

The percentage of commuters that bike to work dropped by half in both the City of Rome and in Floyd County from 2008-2010 to 2010-2012, and the City of Cave Spring has reported no commuters that bike to work over the same period. As of the 2010-2012 American Community Survey (ACS) three-year estimates, Rome had 0.7% bicycle commuters compared to Floyd County with 0.4% commuters that bike to work. Both the City of Rome and Floyd County have a higher percentage of commuters that bike to work than in Georgia overall; however, only the City of Rome has higher bicycling commute rates than the U.S. 2010-2012 three-year estimate.

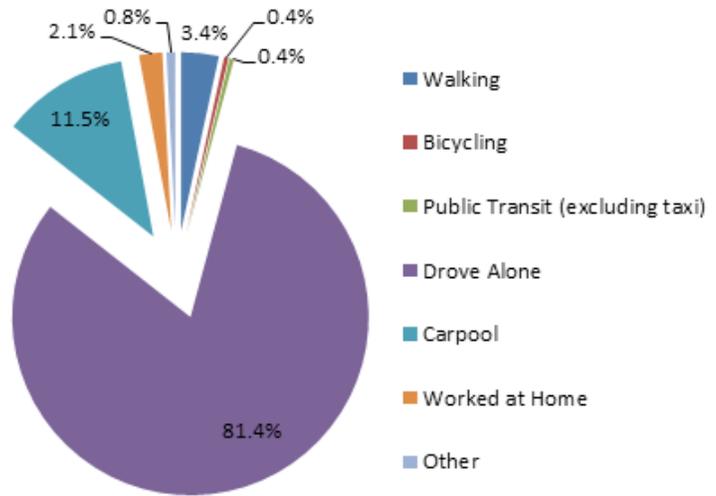
Floyd County has the highest percentage of commuters that walk to work of the five geographic areas considered, with walking percentages increasing from 2.6% in 2008-2010 to 3.4% in 2010-2012. Conversely, in the cities of Rome and Cave Spring, the percentage of commuters that walk to work fell from 2008-2010 to 2010-2012. However, walking to work rates are higher in Rome, Cave Spring, and Floyd County than in Georgia overall.

The following table presents the commute rates for the five geographic areas per the 2008-2010 and 2010-2012 ACS three-year estimates, and the following figure shows the commute rates distribution for Floyd County, Georgia per the 2010-2012 ACS.

Table 2. Commute to Work American Community Survey 3-Year Estimates

Mode	Rome (city)		Cave Spring (city)		Floyd County		Georgia		United States	
	2008-'10	2010-'12	2008-'10	2010-'12	2008-'10	2010-'12	2008-'10	2010-'12	2008-'10	2010-'12
Walking	3.6%	2.2%	3.1%	2.6%	2.6%	3.4%	1.5%	1.6%	2.8%	2.8%
Bicycling	1.5%	0.7%	0.0%	0.0%	0.8%	0.4%	0.2%	0.2%	0.5%	0.6%
Public Transit (excluding taxi)	1.7%	1.1%	0.8%	0.0%	0.9%	0.4%	2.3%	2.1%	5.0%	5.0%
Drove Alone	70.3%	77.8%	78.3%	81.0%	78.5%	81.4%	78.6%	79.4%	10.2%	9.7%
Carpool	17.8%	15.5%	15.4%	14.4%	13.7%	11.5%	11.2%	10.5%	76.0%	76.4%
Worked at Home	3.0%	2.0%	2.4%	2.1%	2.7%	2.1%	4.7%	4.7%	4.2%	4.3%
Other	2.1%	0.7%	0.0%	0.0%	0.9%	0.8%	1.5%	1.4%	1.2%	1.2%

Figure 4. Commute to Work Rates - Floyd County, GA (2010-2012)



## Existing Facilities

Floyd County has some dedicated facilities for those walking and biking, most notably the Heritage Trail System. However, the coverage of the network is low relative to the entire roadway network. Bikeways and multi-use paths cover just 0.85% of the roadway network, and sidewalks cover 5.9% of the roadway network.

Floyd County is unique in that it has mountain bike trails too, including Jackson Hill, parts of the Pinhoti Trail, and Berry College’s mountain bike trail. Access to off-road trails is a new category of facilities considered for Bicycle Friendly Community designation. They provide an opportunity for recreation and physical activity and can be a venue for bicycle events.

Table 3. Rome-Floyd County Existing Facilities Inventory

Facility Type	Miles	% of Total Roadway Miles
Bikeways*	1.9	0.14%
Multi-use Paths**	10	0.71%
Sidewalks	83	5.93%
Roadways	1,400	100.0%

\*Include marked bicycle lanes

\*\*Include paved paths that accommodate walkers and bicyclists

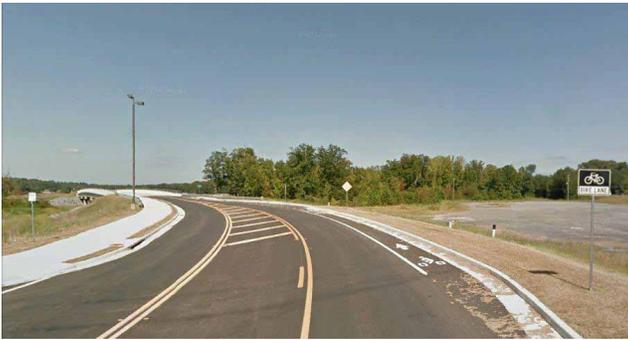
Floyd County also has a network of regional bike routes. These routes are signed but do not have dedicated roadway space for cyclists. For the purpose of calculating existing facilities, only facilities that provide dedicated space for pedestrians and bicyclists were calculated.

## Existing Bicycle Infrastructure

Below is a photo-inventory of typical opportunities and constraints for bicycling found throughout the study area.

### Opportunities

Currently, the only bike lane in Rome-Floyd County is along Armuchee Connector from Oostanaula River and the end of the Heritage Park Trail to State Highway 27. Bicycle parking is available in some areas, such as at schools and Downtown Rome.



*Bike lane along Armuchee Connector*



*Bike parking in Downtown Rome, adjacent to transit station.*



*Bike Fix-It Station in Bridgepoint Plaza in Downtown Rome.*

### Constraints

Current constraints for bicycling include a lack of a network of on-street bikeways. Busy roads without bicycle facilities also present major challenges, not only for riding safely along these roadways but also for simply crossing them.



*Broad Street - No on-street bikeways to or in Downtown Rome.*



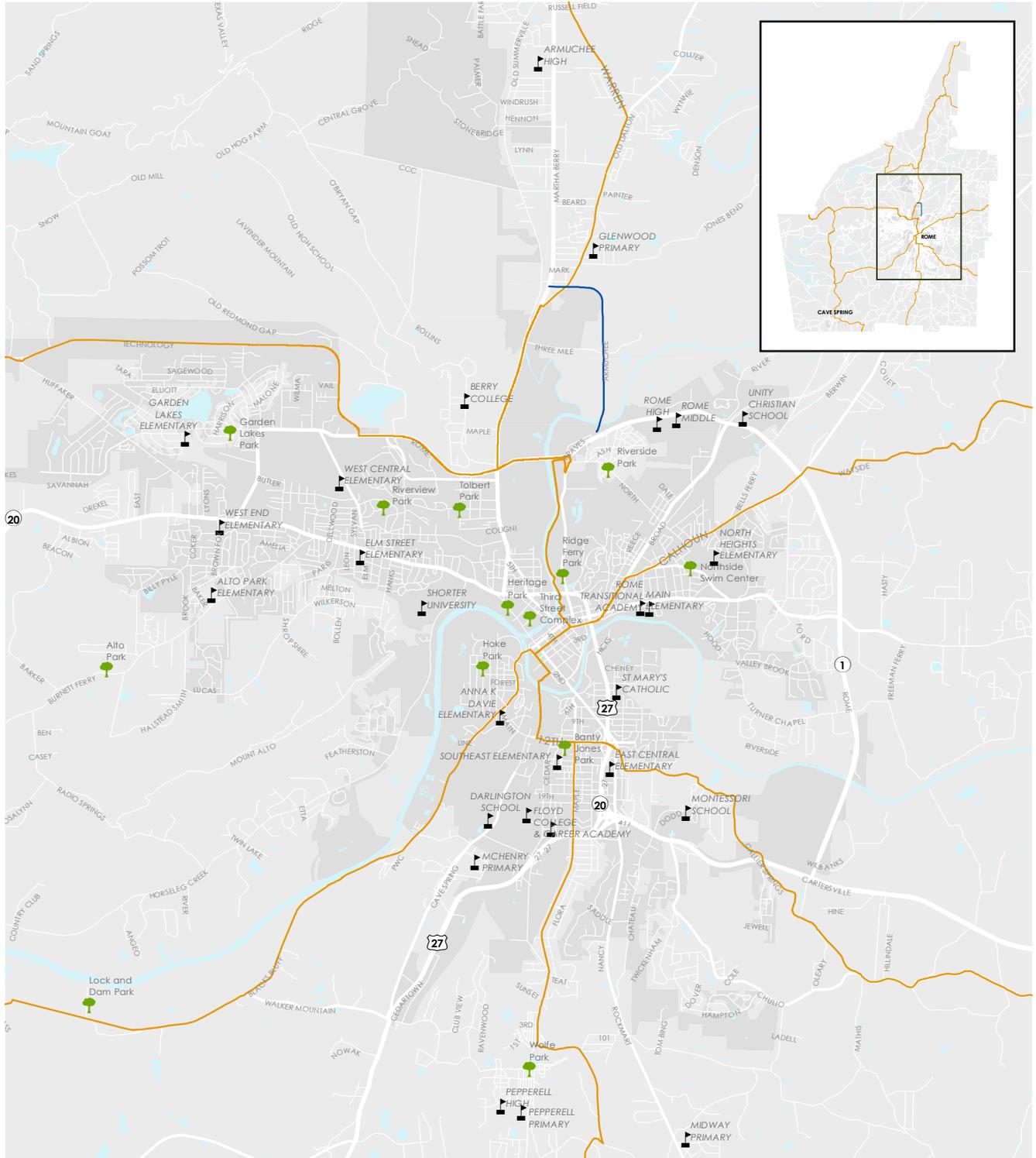
*North 2nd Avenue Bridge - No dedicated on-street bikeways over major bridges in Rome.*



*Shorter Avenue - High volume, high speed corridors create high-stress environments for bicyclists.*



*Broad Street Bridge- Bicyclist riding on the sidewalk. It is illegal to ride a bike on the sidewalk in Rome.*



## Existing On-street Bikeways and Regional Bike Routes

Rome-Floyd County, GA MPO Bicycle and Pedestrian Plan



## Existing Pedestrian Infrastructure

Below is a photo-inventory of typical opportunities and constraints for walking found throughout the study area.

### Opportunities

Sidewalks, marked crosswalks, curb ramps, and signalized intersections provide dedicated space for pedestrians and help people navigate intersections safely.



*Marked crosswalks*



*Sidewalks over major bridges.*



*Curb ramps help make sidewalks accessible for all abilities.*



*Pedestrian signs help mark crosswalks.*

### Constraints

Sidewalk gaps and broken sidewalks can make it difficult to walk along streets, particularly major corridors. Lack of frequent crossings makes it difficult to cross roads, particularly along major corridors.



*Some major streets lack sidewalks on both sides of the street.*



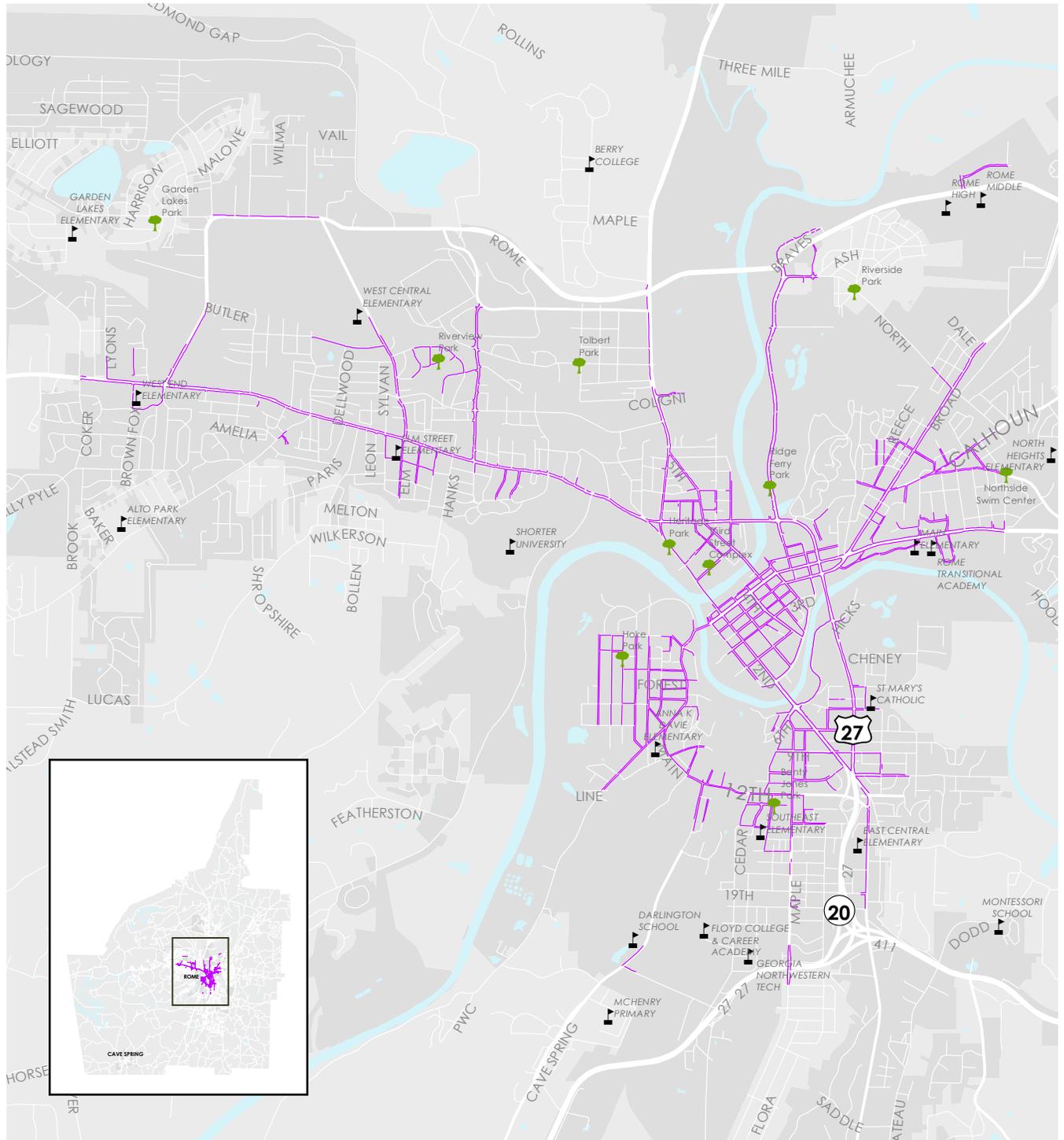
*Some sidewalks abruptly end, creating gaps in the sidewalk network.*



*Some neighborhood streets still lack sidewalks on at least one side of the street.*



*Broken sidewalks or uneven sidewalks can create safety issues for those walking or using a wheelchair.*



## Existing Sidewalks

Rome-Floyd County, GA MPO Bicycle and Pedestrian Plan

- Existing Sidewalk
- Park
- Water Feature
- School
- City Limits
- Floyd County Limits



## Existing Trail Infrastructure

Below is a photo-inventory of typical opportunities and constraints for trails found throughout the study area.

### Opportunities

Trails in Rome and Floyd County allow the community to connect with natural areas, such as the rivers, and connect between neighborhoods. Several pedestrian bridges help pedestrians and bicyclists cross the rivers throughout Downtown Rome.



*Several bicycle and pedestrian only bridges provide river crossings.*



*Wayfinding and interpretive signage provides trail information.*



*Trails provide access to rivers and natural areas.*



*Trails currently connect to several parks.*

### Constraints

Gaps between existing trails limit route options, length of trips, and connections to destinations in the community. The width of trails in many places is narrow, making it difficult or unsafe for pedestrians and bicyclists to pass one another.



*Limited opportunities to cross the rivers outside of Downtown Rome.*



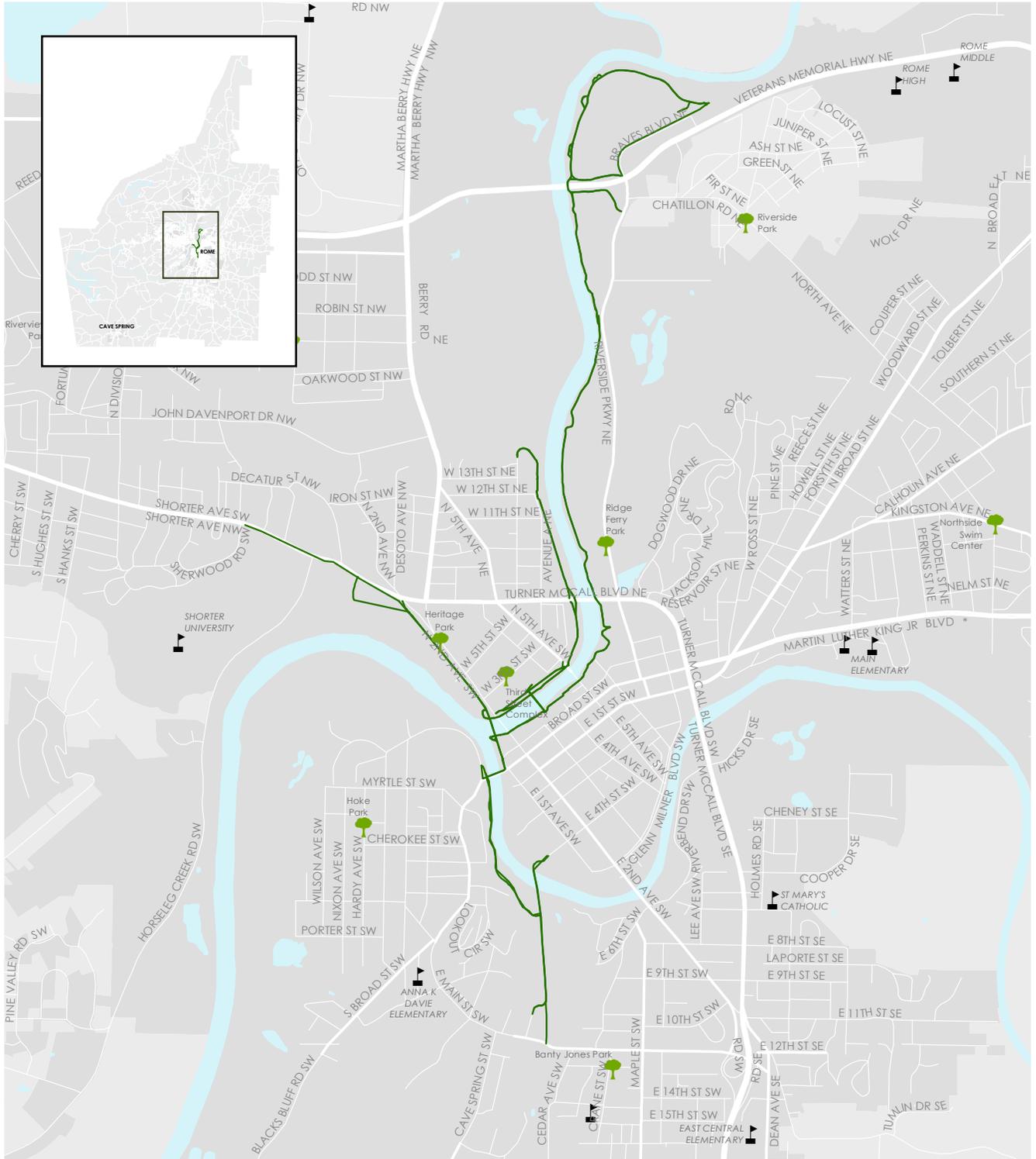
*Narrow sections of trails create bicycle and pedestrian conflicts*



*Wayfinding to destinations from the trail is limited.*



*Access from the trails to Broad Street is limited and not visually intuitive.*



## Existing Trails

Rome-Floyd County, GA MPO Bicycle and Pedestrian Plan

- Existing Trail
- Park
- Water Feature
- School
- City Limits
- Floyd County Limits



# Bicycle Friendly and Walk Friendly Community Assessment

## Overview

The Walk Friendly Community (WFC) program is a national initiative, led by the Pedestrian and Bicycle Information Center (PBIC), intended to encourage communities to improve their local pedestrian environments. Similarly, the Bicycle Friendly Community (BFC) program, led by the League of American Bicyclists, is intended to help communities make bicycling a viable transportation and recreation option regardless of age.

Both programs incorporate assessments that are useful for discovering where a community stands with respect to pedestrian and bicycling facilities and activities. The WFC and BFC assessments recognize existing success in communities that already promote walking and biking as well as provide a framework for those areas trying to achieve higher walking and bicycling rates.

Both assessments address the “Five E’s”: engineering, education, evaluation, enforcement, and encouragement. The engineering category refers to infrastructure-related elements (e.g., bikelanes, sidewalks, ADA accommodations, etc.), while the other four E’s refer to non-infrastructure efforts (such as safety campaigns, planning efforts, etc.). Comprehensive pedestrian and bicycle plans should address all five E’s to effectively advance pedestrian and bicycling activities in a community. Communities seeking status as WFC and BFC must make relevant advances in each of the Five E’s.

## Conclusions

For both walking and biking, Rome-Floyd County has infrastructure, policies, or programs in place to become a WFC or BFC. However, there are significant gaps related to these topics too. Using the WFC scorecard and the BFC scorecard, Rome-Floyd County scored low with each assessment.

Rome-Floyd County is not ready to apply for WFC or BFC, but the county and cities within the county can take significant steps towards implementing the needed improvements to achieve the designation for either program. The recommendations for this Plan were developed using the Five E’s as a framework and identified using community input, steering committee input, and guidance from the consulting team. The recommendations for this Plan, when implemented, will position the Cities of Rome and Cave Spring and Floyd County to apply for and receive WFC and BFC designations.



## WFC/BFC in Georgia

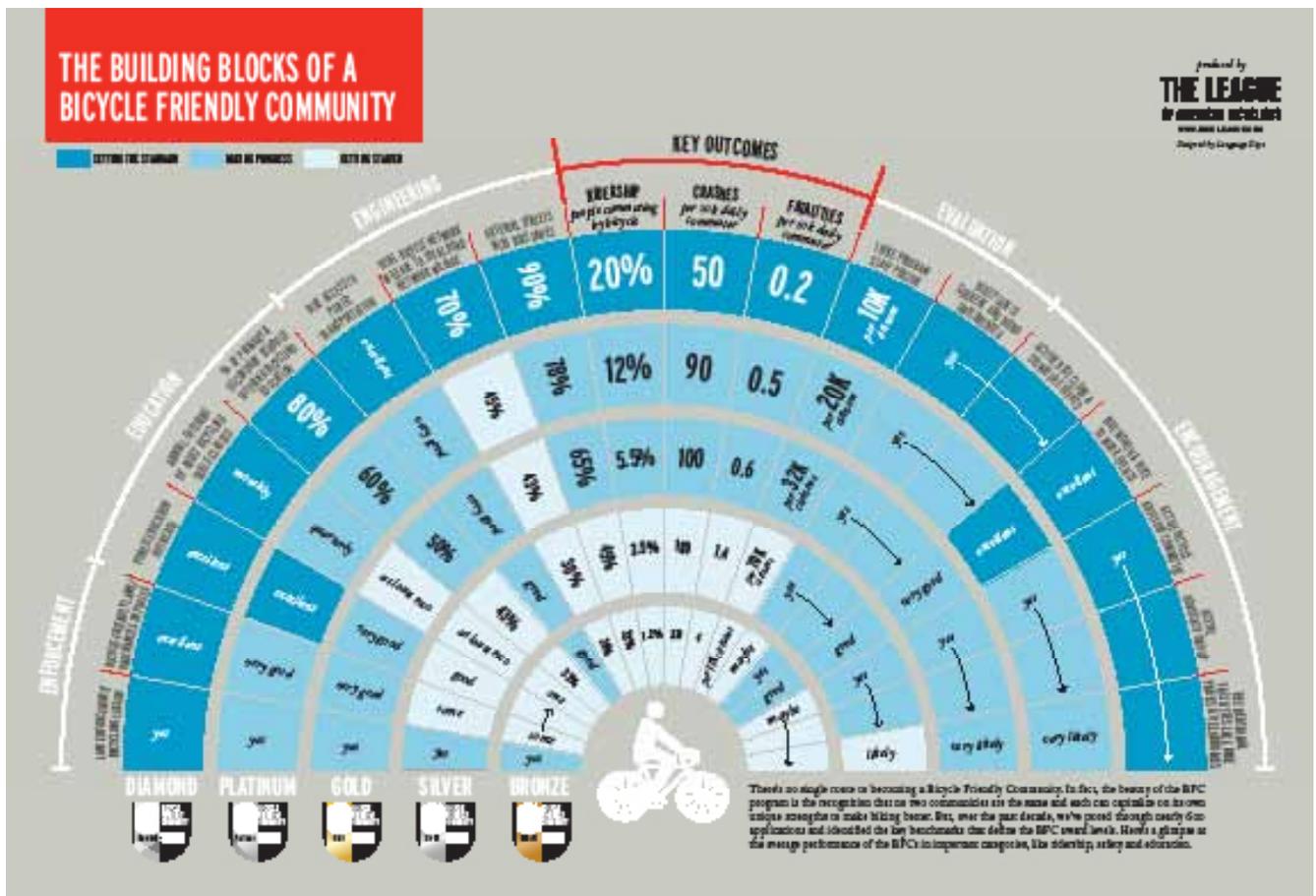
Several communities in Georgia have achieved WFC and BFC status. Seven communities have achieved a Bronze-Level BFC designation; however, the closest BFC community to northwest Georgia is Roswell.

The Georgia BFC communities:

- Athens-Clarke County
- Decatur
- Jekyll Island
- Milledgeville
- Roswell
- Savannah
- Tybee Island

Two communities in Georgia have achieved WFC designation:

- Decatur (Silver-Level)
- Atlanta (Bronze-Level)



BFC Infographic. Download the full version here: <http://bikeleague.org/sites/default/files/BFC%20infographic.pdf>

## Achieving BFC/BFC Scorecard

Communities wishing to become a BFC must submit an application to the League of American Bicyclists that answers questions related to the Five E's and provides other relevant community information. After an application is submitted, a local review is conducted to obtain local feedback and followed with the review by a panel of national bicycle professionals. Communities designed a BFC will receive an award and two Bicycle Friendly Community road signs.

Table 4 provides a quick "scorecard" assessment of existing conditions in Rome- Floyd County, based on key elements of the Bicycle Friendly Community designation criteria. The assessment of each of the Five E's is based on the field observations and research conducted by the consulting team as well as steering committee input.

Rome-Floyd County scored a 4 out of a possible 20 points. Points were counted in the Education, Evaluation, and Encouragement categories. No points were recorded in the Engineering or Enforcement categories. The score shows that Rome-Floyd County has some improvements to make before becoming a designated Bicycle Friendly Community. However, several BFC elements are already in place for Rome-Floyd County, and in a relatively short time frame, Rome-Floyd County can make significant progress towards becoming a BFC.

Table 4. Rome-Floyd County Bicycle Friendly Community Scorecard

Question	Yes	No	Notes
<b>Engineering</b>			
Does your community have a comprehensive, connected, and well-maintained bicycling network?	0	1	
Is bike parking readily available throughout the community?	0	1	
Is there a complete streets ordinance or another policy that mandates the accommodation of bicyclists on all road projects?	0	1	
Does your community require bike lanes to be constructed or upgraded with all (or the majority of) new private development?	0	1	
<b>Engineering Score Total</b>	<b>0/4</b>		
<b>Education</b>			
Is there a community-wide Safe Routes to School (SRTS) Program that includes bicycle education?	1	0	Several schools have coordinated with the Georgia SRTS program.
Are there bicycling education courses available for adults in the community?	0	1	
Does your community educate motorists and cyclists on their rights and responsibilities as road users?	0	1	
<b>Education Score Total</b>	<b>1/3</b>		
<b>Evaluation</b>			
Is there a specific plan or program to reduce bicyclist/motor vehicle crashes?	0	1	
Does your community have a current comprehensive bicycle plan?	1	0	Previous plans, including Rome and Floyd County Trail Facilities Plan 2008 and Long Range Transportation Plan (LRTP) 2040, have addressed bicycling planning.
Is there a bicycle advisory committee that meets regularly?	0	1	
Does your community have a bicycle program manager?	0	1	
Has your community established a connectivity policy, bicycle-friendly block length standards, and connectivity standards for new developments, or convenient bicycle access requirements?	0	1	
<b>Evaluation Score Total</b>	<b>1/6</b>		
<b>Enforcement</b>			
Do law enforcement officers receive training on the rights and responsibilities of all road users?	0	1	
Does your community have law enforcement or other public safety officers on bikes?	0	1	
Do local ordinances promote safety and accessibility for bicyclists?	0	1	
<b>Enforcement Score Total</b>	<b>0/3</b>		
<b>Encouragement</b>			
Does your community have an up-to-date bicycle map?	0	1	
Does the community celebrate bicycling during National Bike Month with community rides, Bike to Work Day, or media outreach?	0	1	Bicycle Rodeo held in March of each year. Bicycle safety helmets were distributed at no cost to over 225 youth in 2014.
Does the community host any major community bicycling events or rides?	1	0	Georgia Cycling Road Race Championship
Is there an active bicycle advocacy group in the community?	1	0	Bike! Walk! Northwest Georgia
<b>Encouragement Score Total</b>	<b>2/4</b>		
<b>Bicycle Friendly Total</b>	<b>4/20</b>		

## Achieving WFC/WFC Scorecard

Communities wishing to become a WFC must apply to Walk Friendly Communities via the online application. The WFC Assessment Tool available from the website includes all of the questions related to the Five E's and other relevant community information needs contained within the application for communities to prepare. After an application is submitted, a multi-person review panel scores the applications, and then WFC award designations are announced.

Table 5 provides a "scorecard" assessment of existing conditions in Floyd County based on key elements of the WFC Assessment Tool. The assessment of each of the Five E's for Rome-Floyd County is based on the field observations and research conducted by the consulting team and input from the steering committee members representing each of Rome, Cave Spring, and Floyd County.

Rome-Floyd County scored a 4 out of a possible 21 points. Points were scored in the Engineering, Education, Encouragement, and Evaluation categories. No points were recorded in the Enforcement category. The score shows that Rome-Floyd County has some improvements to make before becoming a designated Walk Friendly Community. However, several WFC elements are already in place, and in a relatively short time frame, Rome-Floyd County can make significant progress towards becoming a WFC.

Table 5. Rome-Floyd County Walk-Friendly Community Scorecard

Question	Yes	No	Notes
<b>Engineering</b>			
Does your community have a comprehensive, connected, and well-maintained pedestrian network?	1	0	83 miles of existing sidewalks in Rome.
Is there a Complete Streets Ordinance or another policy that mandates the accommodation of pedestrians on all road projects?	0	1	
Has your community adopted an ADA Transition Plan for the public right of way?	0	1	
If yes, provide more info (e.g., what year was the plan adopted, provide a copy of the plan, what has been implemented, etc.)			
Does your community have a policy requiring sidewalks on both sides of arterial streets?	0	1	
Does your community have a policy requiring sidewalks on both sides of collector streets?	0	1	
Does your community require sidewalks to be constructed or upgraded with all (or the majority of) new private development?	0	1	
<b>Engineering Score Total</b>	<b>1/6</b>		
<b>Education</b>			
Is there a community-wide Safe Routes to School Program that includes pedestrian education?	1	0	Several schools have coordinated with the Georgia SRTS program.
Are there pedestrian education courses available for adults in the community?	0	1	
Does your community educate motorists and pedestrians on their rights and responsibilities as road users?	0	1	
<b>Education Score Total</b>	<b>1/3</b>		
<b>Evaluation</b>			
Is there a specific plan or program to reduce pedestrian/motor vehicle crashes?	0	1	
Does your community have a current comprehensive pedestrian plan or pedestrian safety action plan?	0	1	
Is there a pedestrian advisory committee that meets regularly?	0	1	
Does your community have a pedestrian program manager?	0	1	
Has your community established a connectivity policy, pedestrian-friendly block length standards, and connectivity standards for new developments, or convenient pedestrian access requirements?	0	1	
Is your community served by public transit, and if so, what route planning/trip information is provided for transit passengers?	1	0	Rome Transit Department provides Paratransit service.
<b>Evaluation Score Total</b>	<b>1/6</b>		
<b>Enforcement</b>			
Do law enforcement officers receive training on the rights and responsibilities of all road users?	0	1	
Does your community have law enforcement or other public safety officers on foot?	0	1	
Do local ordinances promote safety and accessibility for pedestrians?	0	1	
<b>Enforcement Score Total</b>	<b>0/3</b>		
<b>Encouragement</b>			
Does the community celebrate pedestrians with special events or media outreach?	0	1	
Does the community host any major community pedestrian events?	0	1	
Is there an active pedestrian advocacy group in the community?	1	0	Bike! Walk! Northwest Georgia
<b>Encouragement Score Total</b>	<b>1/3</b>		
<b>Walk Friendly Total</b>	<b>4/21</b>		

## Previous Plans and Policy

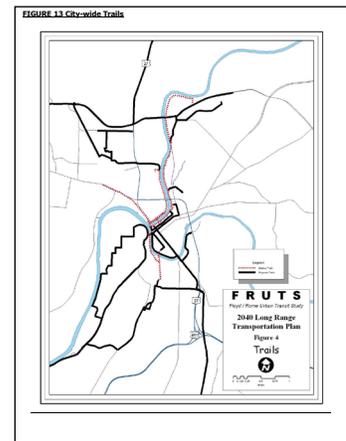
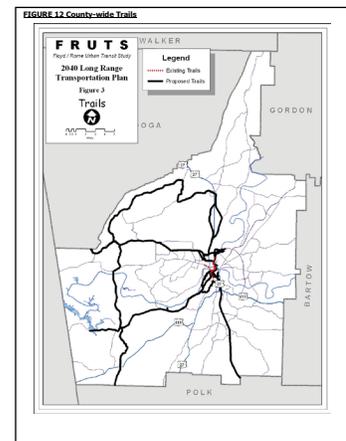
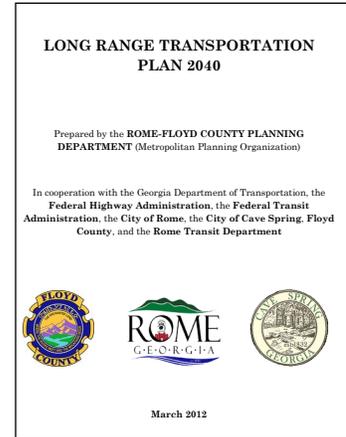
### Long Range Transportation Plan 2040 (March 2012)

The *Long Range Transportation Plan (LRTP) 2040* was prepared by the Rome-Floyd County Metropolitan Planning Organization (MPO) Planning Department and adopted in March 2012. The LRTP provides direction for the Rome-Floyd County MPO related to existing and projected transportation needs and includes an analysis of the current social, economic, and land use characteristics of the area as well as goals and objectives for the future. The LRTP 2040 includes five major goals, one of which contains the following statements:

*“Connect and expand the bike/pedestrian system within the community and connect with regional trail systems, especially the Silver Comet Trail via Rockmart or Cedartown and the Sims Mountain Trail. Encourage the construction and maintenance of sidewalks within and between residential, recreational, educational, and commercial developments.”*

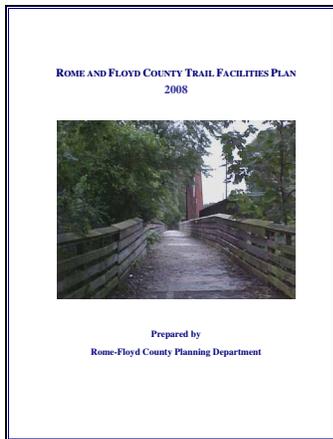
In addition, the LRTP includes a major goal to maintain the existing network and facilities.

The Bicycle/Pedestrian section of the LRTP assesses the existing state of the trails and sidewalks system and discusses the needs for the community. Priority trail projects to increase use, and thus the safety and security of all users, are identified as those that connect existing trails to each other and the sidewalk system to establish an interconnected network for transportation and recreation purposes. The LRTP 2040 provides maps of existing and proposed trails as well as a table summarizing the type of trail, length, and cost of trails projects. Two of these trails projects are included in the list of Illustrative Projects 2012-2040: Construct Pinhoti Connector Trail from Rome to Lyerly and Construct Silver Comet Connector Trail from Rome to Cedartown. As of 2014, neither project has commenced design or construction.



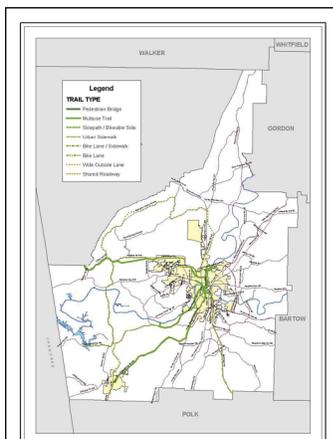
***This Plan has refined the trail network recommendations in the LRTP 2040.***

## Rome and Floyd County Trail Facilities Plan (May 2004 and 2008)

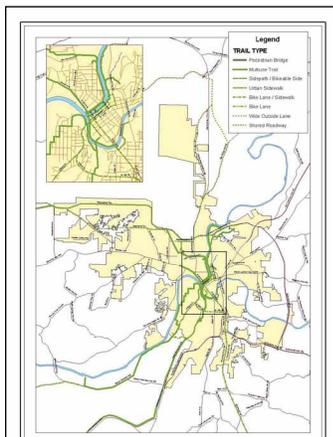


The *Rome and Floyd County Trail Facilities Plan* was published in 2004 and updated in 2008, with the intent to facilitate the development of a trail network in the community. The Plans contain an analysis of the trails network of the time, and includes objectives, suggestions, and maps to act as guides for future trail facilities development in the Rome-Floyd County area.

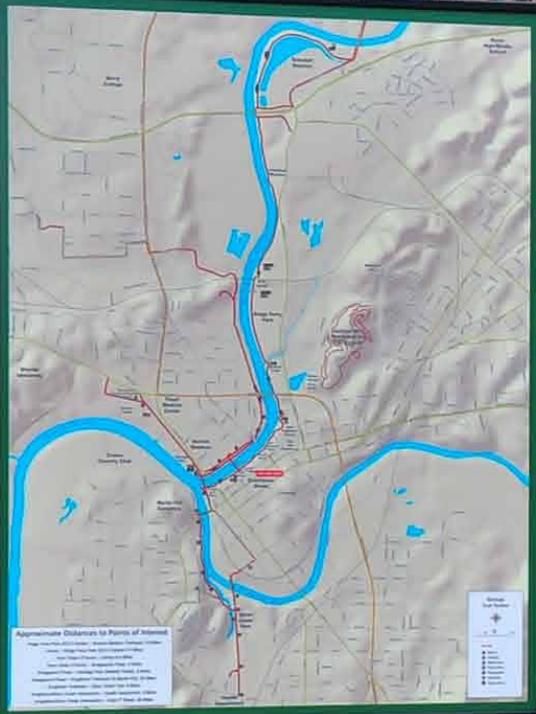
The Trail Facilities Plans propose specific projects related to trail segments and improvements, trailhead facilities, and canoe and water trail facilities as well as recommendations for maintenance, bicycle lanes along major roads and state routes, bicycle lanes and sidewalks on new bridges in Rome, and bicycle and pedestrian facilities in new developments. The Plans include very little discussion on funding the recommendations and do not provide an overview of potential funding sources for trail, bicycle, and/or pedestrian facilities.



Several of the improvements proposed in the Plans have been accomplished, such as the construction of a downtown pedestrian bridge and the addition of some informational signage. However, many improvements remain a priority for many residents in the Rome-Floyd County area, such as creating a connected trail from Rome to Cave Spring and on to the Silver Comet Trail.



***This Plan has refined the trail network recommendations in the Rome and Floyd County Trail Facilities Plan***





## CHAPTER OUTLINE:

- Overview
- Sidewalks and Crossings
- Trails
- On-Street Bikeways
- Support Facilities
- Program Recommendations
- Policy Recommendations

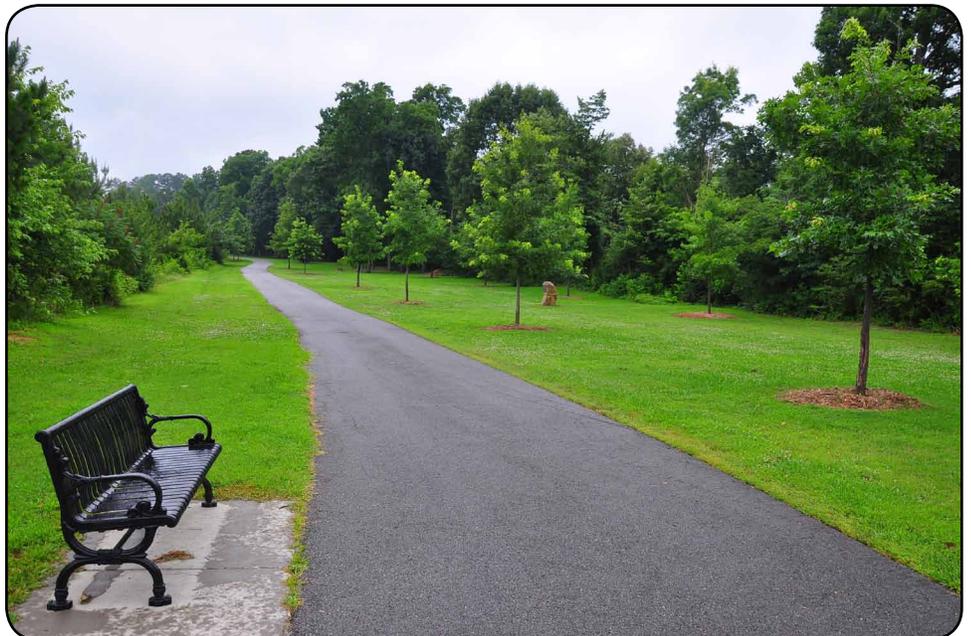
## Overview

A comprehensive set of infrastructure improvements, policy changes, and programs are recommended to increase the safety, convenience, and enjoyment of bicycling, walking, and riding transit in Rome and throughout Floyd County. These recommendations should be implemented in coordination with Floyd County, Cities of Rome and Cave Spring, Rome-Floyd County MPO, and the Georgia Department of Transportation (GDOT) District 6.

Bicycle and pedestrian infrastructure recommendations are presented first, divided into four major sections:

- Sidewalks and Crossings
- Trails
- On-Street Bikeways
- Support Facilities

These sections are followed by a section that focuses on programs and policies that can encourage, enforce, and educate those in the community about walking and biking.



*Improving connectivity between Rome's existing trails is one of the key elements of this Plan.*

# Sidewalks and Crossings

## Overview

The focus of the pedestrian recommendations is on access to parks and schools as well as along major corridors. These areas present the greatest need in terms of safety as well as access to jobs, recreation, and education.

Investments in walking infrastructure in Rome and in Floyd County, including sidewalks, street crossings, and transit stops, should be prioritized when considering funding. Improvements in walking infrastructure will need to be coordinated between different departments, jurisdictions, and property owners depending on the type of facility (e.g., crosswalk versus sidewalk) needed, right-of-way ownership (e.g., city versus county street), and type of project (e.g., maintenance project versus capital project).

The recommendations presented within this section should be followed regardless of the responsible party, facility type, or source of funding.

This section includes several elements that summarize the geographic and design needs of pedestrians:

- Pedestrian Priority Corridors
- Pedestrian Priority Zones
- Intersection Crossings
- Sidewalk Design

These topics are covered in the subsequent sections.



*Well maintained sidewalks along major roads are critical to pedestrian mobility.*

## Priority Pedestrian Area Policies

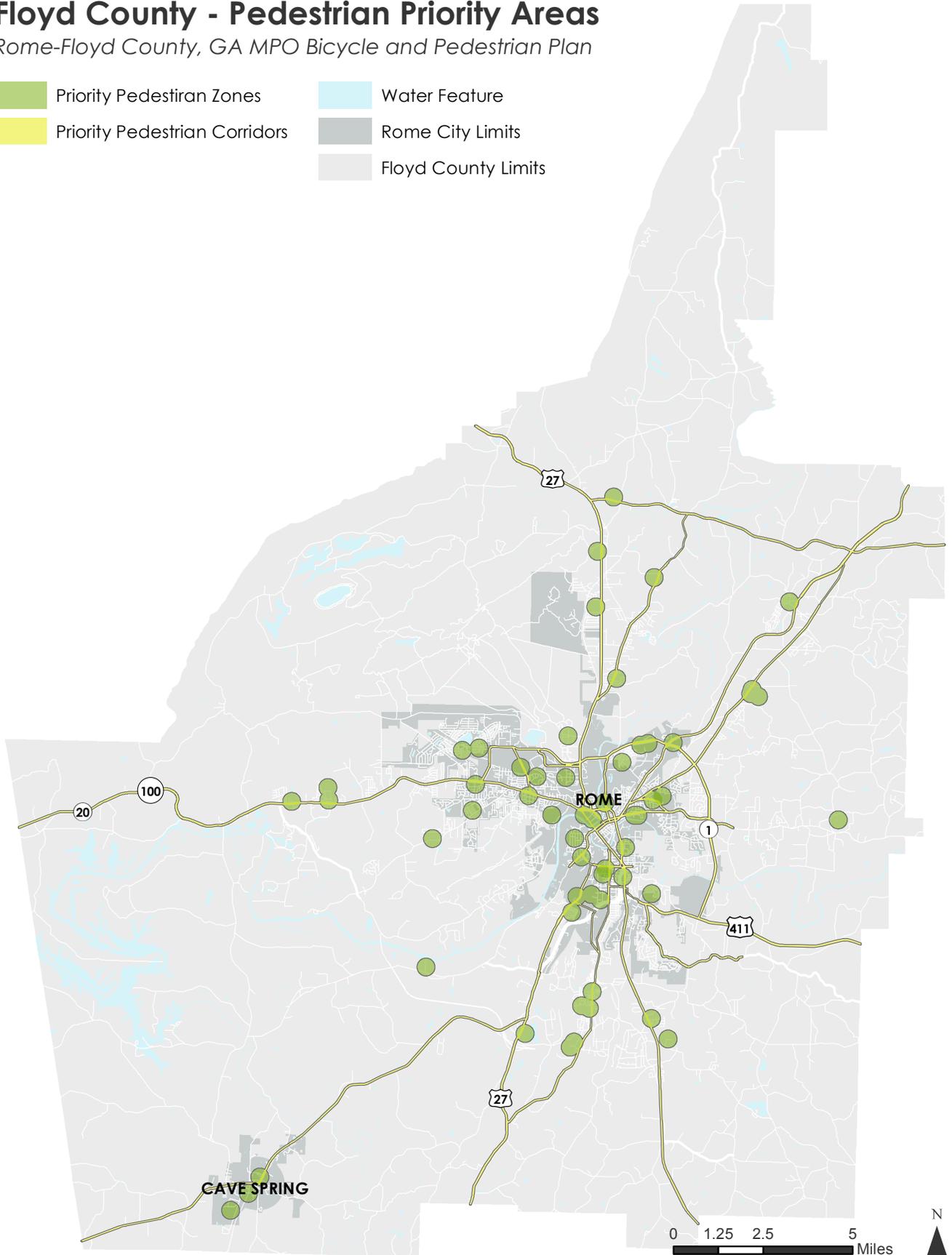
For Pedestrian Priority Corridors and Zones, the following conditions should be provided. If they are not provided, Rome and Floyd County should identify ways to provide them.

- **Gaps in the sidewalk network should be closed.** The sidewalk network should be complete and connected within the Pedestrian Priority areas.
- **Priority pedestrian areas should be universally accessible.** Sidewalks and crossings should be ADA compliant and adequately maintained.
- **Sidewalks should be along a roadway.** Pedestrians should have access and a pathway on a roadway. Along major streets, collector roads, and higher density residential streets, sidewalks should be provided on both sides of the roadway. Otherwise, a sidewalk along at least one side of the roadway should be provided.
- **Frequent and safe street crossings should be provided.** Within Pedestrian Priority Zones and along Pedestrian Priority Corridors, pedestrians should be able to cross streets frequently. Crossings should be marked or signalized to provide a safe crossing.
- **Bus stops should be located in close proximity to street crossings.** Bus stops should be located as close as possible to marked or signalized street crossings to encourage riders to cross the street safely.
- **Bridges and underpasses should provide sidewalks on both sides of the roadway.** Pedestrians should be able to cross under or over railroads, rivers and freeways on both sides of the roadway.

# Floyd County - Pedestrian Priority Areas

Rome-Floyd County, GA MPO Bicycle and Pedestrian Plan

- Priority Pedestrian Zones
- Priority Pedestrian Corridors
- Water Feature
- Rome City Limits
- Floyd County Limits



## Pedestrian Priority Corridors

The Pedestrian Priority Corridors are the arterials, collectors, or major transit corridors in Floyd County. These corridors are characterized by higher vehicle speeds and volumes than on neighborhood streets. Some of these streets are also served by transit. Many of the major destinations and commercial areas are also located along these corridors.

For these corridors, a particular focus should be made on providing frequent and safe crossings, appropriate bus stop space and locations, and access to adjacent neighborhoods and businesses. Also, pedestrian considerations should be done in context with the surrounding development patterns and planned future growth. In areas that are more rural, infrastructure such as intersection crossings and wide shoulders may be appropriate. In areas that are more urban, sidewalks instead of a wide shoulder should be provided.\*

A focus should also be made to coordinate infrastructure improvements with GDOT and the Rome Transit Department. These corridors are maintained and served by multiple jurisdictions and agencies.

## Pedestrian Priority Zones

Similar to priority corridors, Pedestrian Priority Zones have unique needs and should be a top priority when considering investments in pedestrian infrastructure. For these areas, a particular focus should be made on providing frequent and safe crossings as well as reducing vehicle speeds.

## Schools

Children and young adults should be able to walk to school safely. Most students are not old enough to drive and should have the opportunity to get to school by means other than driving.

Additionally, schools are community centers and often have activities going on after regular school hours, both for school-related activities as well as other community events. Adequate sidewalks, frequent crossings, and other safety infrastructure should be provided to accommodate safe access from neighborhoods surrounding schools, particularly for students.

## Parks

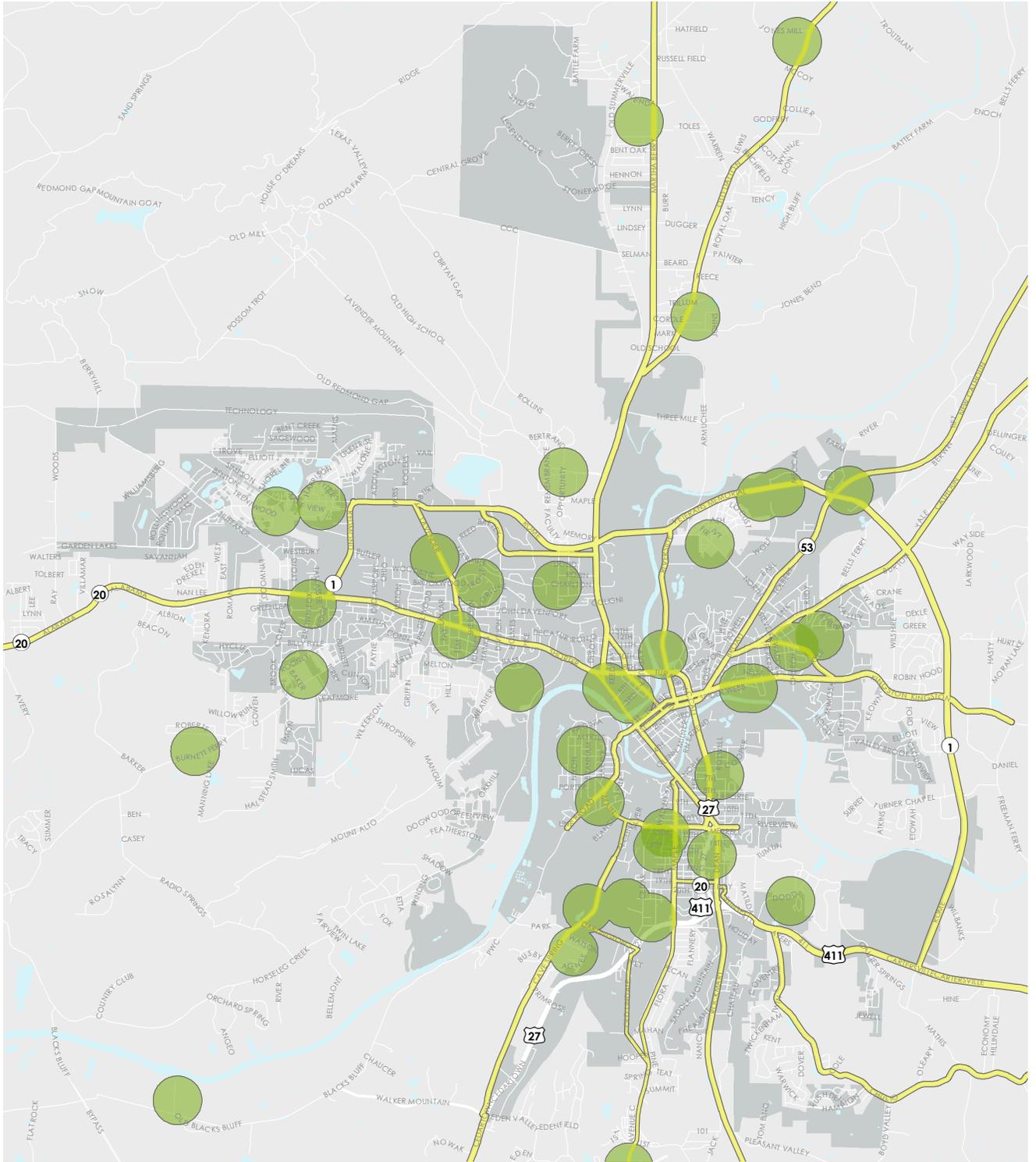
Parks provide recreation opportunities for the community and should be accessible by foot from adjacent neighborhoods. All parks should be accessible from the adjacent neighborhoods by sidewalk.

Consideration should also be given to pedestrian only access points to parks. This strategy can decrease the distance one has to walk to enter a park, which in turn decreases the time it takes to walk to a park.

\* Note: GDOT does not allow pedestrians or non-motorized vehicles on freeways. The reasoning behind this policy is that the high speeds, high volumes, and lack of at-grade intersections along this type of route are not appropriate for providing safe crossings or bus stop space. Any such facilities would need to be provided on parallel and barrier separated corridors. No pedestrian or non-motorized vehicle facilities are proposed on freeways as part of this plan.



**Commercial areas along major streets, schools, and parks make up the core elements of the Priority Pedestrian Areas.**



## City of Rome - Priority Pedestrian Areas

Rome-Floyd County, GA MPO Bicycle and Pedestrian Plan



- Priority Pedestrian Zones
- Priority Pedestrian Corridors
- Water Feature
- City Limits
- Floyd County Limits

## Implementing Pedestrian Priority Zones

To adequately assess the needs along the Pedestrian Priority Corridors and Priority Pedestrian Zones, the following implementation strategies should be employed:

- **Inventory.** The City of Rome and Floyd County should inventory the pedestrian facilities, including the location and width of sidewalks along with the location of marked crossings. The inventory should be maintained in GIS and regularly updated based on maintenance and construction projects.
- **Safety Audits.** The City of Rome and Floyd County should conduct regular safety audits along priority corridors and within priority zones to assess the unique safety needs and appropriate safety improvements. See the Programs and Policy section of this chapter for more information on Safety Audits.

## Intersections and Crossings

Attributes of pedestrian-friendly intersection intersections include:

- **Clear Space:** Corners should be clear of obstructions. They should also have enough room for curb ramps, for transit stops where appropriate, and for street conversations where pedestrians might congregate.
- **Visibility:** It is critical that pedestrians on the corner have a good view of vehicle travel lanes and that motorists in the travel lanes can easily see waiting pedestrians.
- **Legibility:** Symbols, markings, and signs used at corners should clearly indicate what actions the pedestrian should take.
- **Accessibility:** All corner features, such as curb ramps, landings, call buttons, signs, symbols, markings, and textures, should meet accessibility standards and follow universal design principles.
- **Separation from Traffic:** Corner design and construction should be effective in discouraging turning vehicles from driving over the pedestrian area. Crossing distances should be minimized.
- **Lighting:** Adequate lighting is an important aspect of visibility, legibility, and accessibility.

These attributes will vary with context but should be considered in all design processes. For example, suburban and rural intersections may have limited or no signing. However, legibility regarding appropriate pedestrian movements should still be taken into account during design.



*Safety audits should be conducted along pedestrian priority corridors to assess conditions and needs.*



*High visibility crosswalk markings clearly indicate where drivers should expect pedestrians in the roadway.*

## Crossing Selection

The specific type of treatment at a crossing may range from a simple marked crosswalk to full traffic signals or grade separated crossings. Crosswalk lines should not be used indiscriminately, and appropriate selection of crossing treatments should be evaluated in an engineering study and be performed before a marked crosswalk is installed. The engineering study should consider the number of lanes, the presence of a median, the distance from adjacent signalized intersections, the pedestrian volumes and delays, the average daily traffic (ADT), the posted or statutory speed limit or 85th-percentile speed, the geometry of the location, the possible consolidation of multiple crossing points, the availability of street lighting, and other appropriate factors.

PEDESTRIAN CROSSING CONTEXTUAL GUIDANCE	Local Streets 15-25 mph		Collector Streets 25-30 mph			Arterial Streets 30-45 mph							
	2 lane	3 lane	2 lane	2 lane with median refuge	3 lane	2 lane	2 lane with median refuge	3 lane	4 lane	4 lane with median refuge	5 lane	6 lane	6 lane with median refuge
Crosswalk Only (high visibility)	✓	✓	EJ	EJ	X	EJ	EJ	X	X	X	X	X	X
Crosswalk with warning signage and yield lines	EJ	✓	✓	✓	✓	EJ	EJ	EJ	X	X	X	X	X
Active Warning Beacon (RRFB)	X	EJ	✓	✓	✓	✓	✓	✓	X	✓	X	X	X
Hybrid Beacon	X	X	EJ	EJ	EJ	EJ	✓	✓	✓	✓	✓	✓	✓
Full Traffic Signal	X	X	EJ	EJ	EJ	EJ	EJ	EJ	✓	✓	✓	✓	✓
Grade separation	X	X	EJ	EJ	EJ	X	EJ	EJ	EJ	EJ	EJ	✓	✓

LEGEND	
Most Desirable	✓
Engineering Judgement	EJ
Not Recommended	X

## Marked Crossings



### Marked Crosswalks

A marked crosswalk signals to motorists that they must stop for pedestrians and encourages pedestrians to cross at designated locations. Installing crosswalks alone will not necessarily make crossings safer especially on multi-lane roadways.

At mid-block locations, crosswalks can be marked where there is a demand for crossing and there are no nearby marked crosswalks.



### Crosswalk with Warning Signage

To enhance visibility of pedestrians and to encourage vehicles and other roadway users to yield or stop for pedestrians in a crosswalk, warning signs and yield lines can be added in advance of the crosswalk. The type and location of the sign should be selected based on the context and standards for the street being considered.

## Warning Beacons, Signalized Intersections, and Grade Separation



### Active Warning Beacon

Active warning beacons are user activated illuminated devices designed to increase motor vehicle yielding compliance at crossings of multi-lane or high volume roadways.

Types of active warning beacons include conventional circular yellow flashing beacons, in-roadway warning lights, or Rectangular Rapid Flash Beacons (RRFB). Of these, RRFBs have been shown to produce the highest level of compliance.



### Pedestrian Hybrid Beacon

A hybrid beacon consists of a signal-head with two red lenses over a single yellow lens on the major street, and pedestrian and/or bicycle signal heads for the minor street. There are no signal indications for motor vehicles on the minor street approaches.

Hybrid beacons are used to improve non-motorized crossings of major streets in locations where side-street volumes do not support installation of a conventional traffic signal (or where there are concerns that a conventional signal will encourage additional motor vehicle traffic on the minor street). Hybrid beacons may also be used at mid-block crossing locations.



### Full Traffic Signal

At intersections with full traffic signals, pedestrian signal heads should be installed, and the pedestrian signal should be synchronized to coordinate crossing with vehicle timing. Signal phasing should provide adequate time for pedestrians of all ages and abilities to cross safely. Countdown-style signal heads are preferred.



### Grade Separation

In unique situations, grade separation may be required for pedestrians to safely cross to destinations. Bridges help by linking areas separated by barriers such as waterways or interstates.



*The walking environment on Broad Street in Downtown Rome is excellent.*



*Curb ramps built to American Accessibility Act Standards allow people using mobility devices to easily navigate.*

## Sidewalk Design

### General Design

Sidewalks are the most fundamental element of the walking network, as they provide an area for pedestrian travel that is separated from vehicle traffic. Sidewalks are typically constructed out of concrete and are separated from the roadway by a curb or gutter and sometimes a landscaped planting strip area. Sidewalks are a common application in both urban and suburban environments.

Attributes of well-designed sidewalks include the following:

- **Accessibility:** A network of sidewalks should be accessible to all users.
- **Adequate width:** Two people should be able to walk side-by-side and pass a third comfortably. Different walking speeds should be possible. In areas of intense pedestrian use, sidewalks should accommodate the high volume of walkers.
- **Safety:** Design features of the sidewalk should allow pedestrians to have a sense of security and predictability. Sidewalk users should not feel they are at risk due to the presence of adjacent traffic.
- **Continuity:** Walking routes should be obvious and should not require pedestrians to travel out of their way unnecessarily.
- **Landscaping:** Plantings and street trees should contribute to the overall psychological and visual comfort of sidewalk users, and be designed in a manner that contributes to the safety of people.
- **Drainage:** Sidewalks should be well graded to minimize standing water.
- **Social space:** There should be places for standing, visiting, and sitting. The sidewalk area should be a place where adults and children can safely participate in public life.
- **Quality of place:** Sidewalks should contribute to the character of neighborhoods and business districts.

### Widths

The width and design of sidewalks will vary depending on street context, functional classification, and pedestrian demand. Below are preferred widths of sidewalks according to general street type. Standardizing sidewalk guidelines for different areas of the city, dependent on the above listed factors, ensures a minimum level of quality for all sidewalks.

- Local Streets: 4-6 feet
- Commercial Areas: 6-12 feet
- Arterials and Collectors: 4-8 feet

Note that six feet enables two pedestrians (including wheelchair users) to walk side-by-side or to pass each other comfortably.

## Amenities

A variety of streetscape elements can define the pedestrian realm, offer protection from moving vehicles, and enhance the walking experience. Key features are presented below.

- **Street trees:** In addition to their aesthetic and environmental value, street trees can slow traffic and improve safety for pedestrians. Trees add visual interest to streets and narrow the street’s visual corridor, which may cause drivers to slow down. It is important that trees do not block light or the vision triangle.
- **Street furniture:** Providing benches at key rest areas and viewpoints encourages people of all ages to use the walkways by ensuring that they have a place to rest along the way. Benches should be 20” tall to accommodate elderly pedestrians comfortably.
- **Green features:** Green stormwater strategies may include bioretention swales, rain gardens, tree box filters, and pervious pavements (pervious concrete, asphalt, or pavers). Bioswales are natural landscape elements that manage water runoff from a paved surface. Plants in the bioswale trap pollutants and silt from entering a river system.
- **Lighting:** Pedestrian-scale lighting improves visibility for both pedestrians and motorists, particularly at intersections. Pedestrian-scale lighting can provide a vertical buffer between the sidewalk and the street, defining pedestrian areas. Pedestrian-scale lighting should be used in areas of high pedestrian activity.



*Street trees and green features add visual appeal to the street for all users.*



# Trails

## Overview

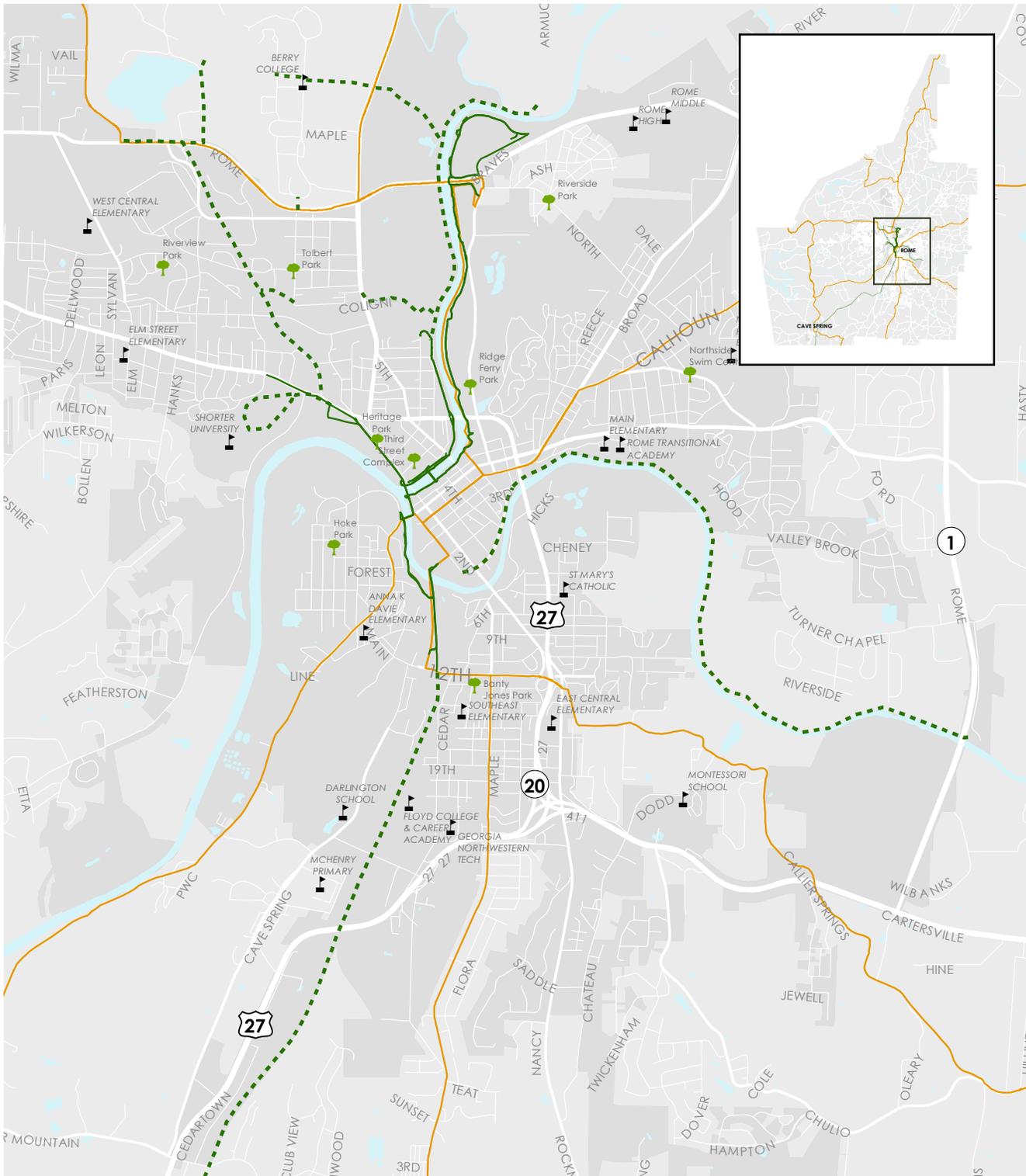
The overall goal of the trails recommendations is to increase connectivity between existing trails within Rome and in the region and to connect colleges and universities with Downtown Rome. Proposed trails in the City of Rome will provide an off-street alternative to on-street bikeways as well as provide a network that can accommodate all ages and abilities. Additionally, the vision for the proposed regional trail from Cave Spring to Rome is that it will eventually connect to the Silver Comet Trail in Cedartown, spurring recreational and economic development opportunities.

The trail recommendations combine recreation and transportation enhancements into one investment. The trails and sidepaths can be a recreation or transportation facility by themselves as well as increase access to parks or other daily destinations in Rome and throughout Floyd County.

## Network Recommendations

The trail network includes a combination of trails, which are completely separate from a roadway, and sidepaths, which are parallel to a roadway. The trail recommendations for this Plan include over 34 miles of new trails and sidepaths. At full build out of the proposed trail network, Rome-Floyd County will have over 44 miles of trails, improving connections to neighborhoods, schools, parks, shopping, and jobs.

The recommendations for each trail segment were carefully selected based on the current trail network, desired community destinations, access to parks, schools, and natural areas, and recreation benefits, among others. Trail segment details are presented in Table 6.



## Existing and Proposed Trails

Rome-Floyd County, GA MPO Bicycle and Pedestrian Plan

- - - Proposed Trail
- Existing Trail
- Regional Bike Route
- Park
- School
- Water Feature
- City Limits
- Floyd County Limits

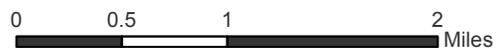


Table 6. Proposed Trail Projects

Trail Project	Project Name	Project Segment	Trail Type	From	To	Length (Miles)
1	Redmond Circle Trail Connection	N/A	Sidepath	Technology Pkwy	Batthey Cir	0.37
2	Berry College to the River - North	A	Sidepath	Remembrance Rd	Martha Berry Blvd	0.56
2	Berry College to the River - North	B	Independent ROW	Martha Berry Hwy	Etowah River Trail - North	0.6
3	Etowah River Trail - North	N/A	Independent ROW	Armuchee Connector	Railroad Bridge near Post Office	1.89
4	Berry College / Shorter University Rai- to-Trail	N/A	Independent ROW	West side of Berry College	North side of Shorter University	2.46
5	Little Dry Creek Trail Connection	N/A	Independent ROW	Berry College / Shorter University Rail-to-Trail	Berry College to the River Trail - South	0.23
6	Berry College to the River - South	A	Independent ROW	Opportunity Dr	Timothy Ave	1.01
6	Berry College to the River - South	C	Independent ROW	Martha Berry Blvd	Railroad Bridge near Post Office	0.11
7	Shorter College Loop Trail	N/A	Independent ROW	Heritage Park Trail near American Legion	Heritage Park Trail near American Legion	1.08
8	Glenn Milner Blvd Rail-to-Trail Conversion	N/A	Independent ROW	1st Ave	8th Ave	0.64
9	Etowah River Trail - South	N/A	Independent ROW	Existing Trail	8th Ave	4.14
10	Cave Spring Trail	A	Independent ROW	12th St	Cedartown Hwy	4.81
10	Cave Spring Trail	B	Sidepath	Cedartown Hwy	Padlock Mountain Rd (Cave Spring)	9.5
10	Cave Spring Trail	C	Sidepath	Padlock Mountain Rd (Cave Spring)	Silver Comet Trail (Cedartown)	10.06

## Trail Design

A shared-use path allows for two-way, off-street bicycle use and also may be used by pedestrians, skaters, wheelchair users, joggers, and other non-motorized users. These facilities are frequently found in parks, along rivers, beaches, and in greenbelts or utility corridors where there are few conflicts with motorized vehicles. Path facilities can also include amenities such as lighting, signage, and fencing (where appropriate).

Key features of shared-use paths include:

- Frequent access points from the local road network
- Directional signs to direct users to and from the path
- A limited number of at-grade crossings with streets or driveways
- Terminating the path where it is easily accessible to and from the street system
- Separate treads for pedestrians and bicyclists when heavy use is expected



### Neighborhood Accessways

- Neighborhood accessways provide residential areas with direct bicycle and pedestrian access to parks, trails, greenspaces, and other recreational areas. They most often serve as small trail connections to and from the larger trail network, typically having their own rights-of-way and easements.
- Additionally, these smaller trails can be used to provide bicycle and pedestrian connections between dead-end streets, cul-de-sacs, and access to nearby destinations not provided by the street network.



### Trails

- Trails are shared-use paths that are separate from a roadway and follow natural features, such as a water way or ridge, utility corridors, such as a power line easement, or along a railroad corridor, such as a rail-with-trail route. These corridors offer excellent transportation and recreation opportunities, particularly for recreation and users of all skill levels preferring separation from traffic.



### Sidepaths

- Shared-use paths along roadways, also called sidepaths, are a type of path that run adjacent to a street. Because of operational concerns, it is generally preferable to place paths within independent rights-of-way away from roadways. However, there are situations where existing roads provide the only corridors available.
- Along roadways, these facilities create a situation where a portion of the bicycle traffic rides against the normal flow of motor vehicle traffic and can result in wrong-way riding where bicyclists enter or leave the path.
- The *AASHTO Guide for the Development of Bicycle Facilities* cautions practitioners of the use of two-way sidepaths on urban or suburban streets with many driveways and street crossings.

## Trail Crossings

At-grade roadway crossings can create potential conflicts between path users and motorists; however, well-designed crossings can mitigate many operational issues and provide a higher degree of safety and comfort for path users. This is evidenced by the thousands of successful facilities around the United States with at-grade crossings. In most cases, at-grade path crossings can be properly designed to provide a reasonable degree of safety and can meet existing traffic and safety standards. Path facilities that cater to bicyclists can require additional considerations due to the higher travel speed of bicyclists versus pedestrians.

Consideration must be given to adequate warning distance based on vehicle speeds and line of sight, with the visibility of any signs absolutely critical. Directing the active attention of motorists to roadway signs may require additional alerting devices such as a flashing beacon, roadway striping, or changes in pavement texture. Signing for path users may include a standard "STOP" or "YIELD" sign and pavement markings, possibly combined with other features such as bollards or a bend in the pathway to slow bicyclists. Care must be taken not to place too many signs at crossings lest they begin to lose their visual impact.

A number of striping patterns have emerged over the years to delineate path crossings. A median stripe on the path approach will help to organize and warn path users. Crosswalk striping is typically a matter of local and state preference and may be accompanied by pavement treatments to help warn and slow motorists. In areas where motorists do not typically yield to crosswalk users, additional measures may be required to increase compliance.



### Midblock Trail Crossings

- Midblock crossings can range from marked, signalized intersection crossings to full signal crossings.
- Crossings typically consist of marked crossing areas, signage, and other markings to slow or stop traffic.
- The approach to designing crossings at mid-block locations depends on an evaluation of vehicular traffic, line of sight, pathway traffic, use patterns, vehicle speed, road type, road width, and other safety issues such as proximity to major attractions.
- When space is available, using a median refuge island can improve user safety by providing pedestrians and bicyclists space to perform the safe crossing of one side of the street at a time.



### Trail Crossings at Intersections

- Signalized crossings provide the most protection for crossing path users through the use of a red-signal indication to stop conflicting motor vehicle traffic.
- A full traffic signal installation treats the path crossing as a conventional four-way intersection and provides standard red-yellow-green traffic signal heads for all legs of the intersection.
- Pedestrian signals are used to indicate when bicyclists and pedestrians should cross, and crosswalk markings should be used to establish where trail users should cross the intersection.

# On-street Bikeways

## Overview

The overall goal with the on-street bikeway recommendations is to create a connected network of bikeways in Rome that facilitates bicycling for transportation and recreation. This means connecting destinations such as schools (including colleges and universities), parks, and commercial districts in addition to facilitating access to existing rural bike routes used primarily for recreational rides. Notably, the on-street bikeways proposed in this Plan will allow people to safely and comfortably ride a bike over most Rome bridges and conveniently access businesses on Broad Street in Downtown Rome.

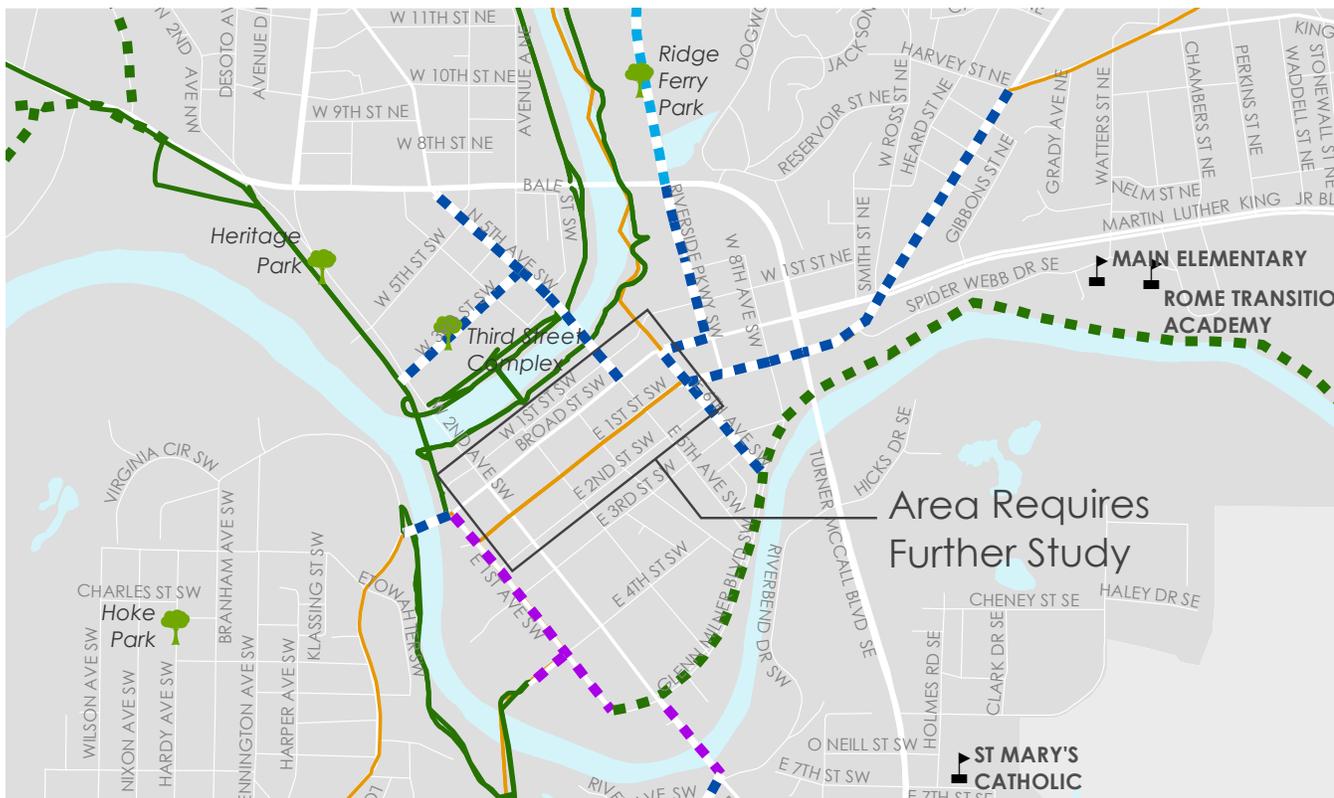
## Network Recommendations

The bikeway recommendations for this Plan include over 24 miles of new on-street bikeways (including bike lanes, buffered bike lanes, cycle tracks, and bicycle boulevards) to dramatically increase Rome-Floyd County's bicycle facility network connectivity. The recommendations were developed to lay the groundwork for a comprehensive, safe, and logical network that connects Downtown Rome to the area's colleges and universities, schools, parks, and surrounding neighborhoods.

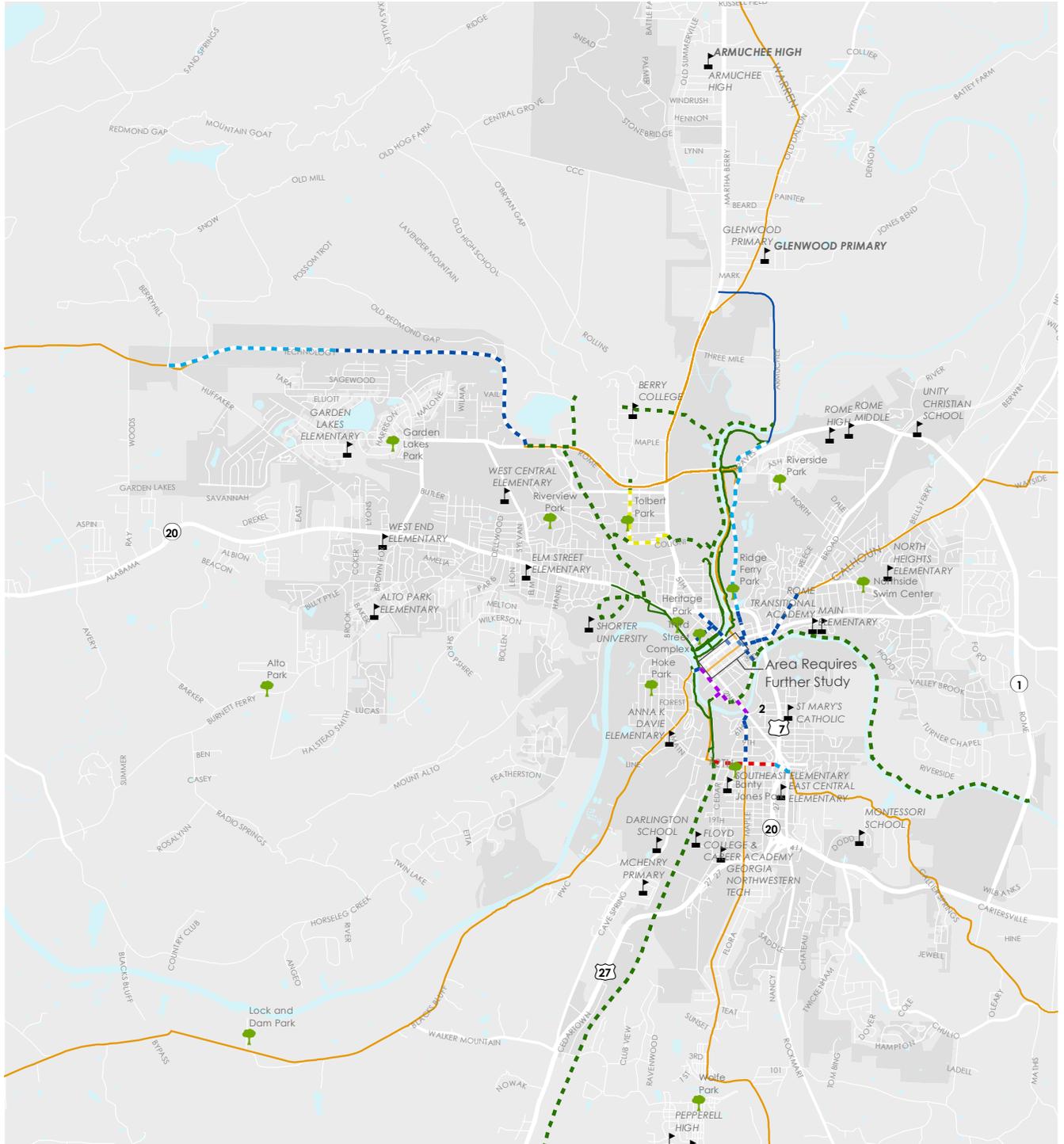
The recommendations for each roadway were carefully selected based on current roadway characteristics as well as design considerations such as volumes and speeds. On-street bikeway segment details are presented in Table 7.



*Bike lanes are one type of on-street bikeway.*



**Downtown Rome Inset Map - see legend on page 34 for bikeway types**



## Existing and Proposed Bikeways

Rome-Floyd County, GA MPO Bicycle and Pedestrian Plan

### Proposed Bikeways

- ..... Bike Lane
- ..... Buffered Bike Lane
- ..... Bicycle Boulevard
- ..... Uphill Bike Lane / Downhill Shared Lane Markings
- ..... Trail
- ..... Cycle Track

### Existing Bikeways

- ..... Trail
- ..... Bike Lane
- ..... Regional Bike Route

- Park
- School
- Water Feature
- City Limits
- Floyd County Limits



Table 6. Proposed On-street Bikeway Projects

On-street Bikeway Project	Project Name	Project Segment	Facility Type	From
1	Technology Pkwy	A	Buffered Bike Lane	Berryhill Rd
1	Technology Pkwy	B	Bike Lane	Berryhill Rd
2	Berry College to the River - South	B	Bicycle Boulevard	Proposed bike/ped bridge over State Loop 1
3	Riverside Pkwy/Braves Blvd	A	Buffered Bike Lane	Heritage Park Trail / Braves parking lot driveway
3	Riverside Pkwy/Braves Blvd	B	Bike Lane	Turner McCall Blvd
3	Riverside Pkwy/Braves Blvd	C	Bike Lane	Village Shopping Center rear driveway
3	Riverside Pkwy/Braves Blvd	D	Bike Lane	Rome Floyd Public Library parking lot driveway
4	3rd St	A	Bike Lane	Heritage Park Trail
4	3rd St	B	Bike Lane	Barron Stadium/Tennis Ctr Pkg lot entrance
4	3rd St	C	Bike Lane	100' SW of 4th Ave
5	5th Ave	A	Bike Lane	3rd St
5	5th Ave	B	Bike Lane	3rd St
5	5th Ave	C	Bike Lane	Bale St
6	1st St/Broad St	A	Bike Lane	6th Ave
6	1st St/Broad St	B	Bike Lane	Riverside Pkwy
6	1st St/Broad St	C	Bike Lane	8th Ave
6	1st St/Broad St	D	Bike Lane	Turner McCall Blvd
6	1st St/Broad St	E	Bike Lane	MLK Jr Blvd
7	Broad St	A	Bike Lane	Branham Ave
7	Broad St	N/A	Requires further study	1st Ave
7	Broad St	B	Bike Lane	6th Ave
8	6th Ave	A	Bike Lane	Broad St
8	6th Ave	B	Bike Lane	Broad St
8	6th Ave	C	Bike Lane	3rd St
9	1st Ave	N/A	Cycle Track	Broad St
10	4th St	N/A	Cycle Track	Silver Creek Trail
11	2nd Ave/6th St /Maple St	A - 2nd Ave	Cycle Track	Glenn Milner Blvd
11	2nd Ave/6th St /Maple St	B - 6th St	Bike Lane	2nd Ave
11	2nd Ave/6th St /Maple St	C - Maple St Part 1	Shared Lane Markings	6th St
11	2nd Ave/6th St/Maple St	D - Maple St Part 2	Bike Lane	Railroad Tracks
12	12th St	A	Uphill Bike Lane, Downhill Shared Lane Markings	Proposed Cave Springs Trail Rail w/trail segment
12	12th St	B	Buffered Bike Lane	2nd Ave SW

To	Implementation Strategy	Proposed Cross Section	Length (Miles)
End of Center Median	Lane Reconfiguration	N->S: 6' BL, 4' Buf, 14' TL, 24' M/CTL, 14' TL, 4' Buf, 6'BL	2.68
Redmond Cir	Lane Reconfiguration	N->S: 6'BL, 12' TL, 12' CTL, 12' TL, 6' TL	1.77
Martha Berry Blvd	Bicycle Boulevard	Remove centerline, add speed humps and SLMs	0.53
Village Shopping Center rear driveway	Lane Reconfiguration	W->E: 6' BL, 4' Buf, 12' TL, 22' M/CTL, 12' TL, 4' Buf, 6' BL	0.14
Village Shopping Center rear driveway	Lane Reconfiguration	W->E: 6' BL, 15' TL, 14' CTL, 15' TL, 6' BL	1.94
Rome Floyd Public Library parking lot driveway	Lane Reconfiguration	W->E: 12' RTL, 6' BL, 12' TL, 12' TL, 6' BL	0.06
Broad St	Lane Reconfiguration	W->E: 5' BL, 2 10' TL, 5'BL	0.07
Barron Stadium/Tennis Ctr Pkg lot entrance	Lane Reconfiguration	NW->SE: 6' BL, 2 12' TLs, 6'BL	0.13
100' SW of 4th Ave	Lane Reconfiguration	NW->SE: 6' BL, 12' TL, 12' CTL, 12' TL, 6' BL	0.08
5th Ave	Lane Reconfiguration	NW->SE: 6' BL, 2 12' TLs, 6' BL	0.08
3rd St	Lane Reconfiguration	W->E: 6' BL, 2 12' TLs, 6' BL	0.2
Bale St	Lane Reconfiguration	W->E: 15' B-IAPL, 6'BL, 2 12'TLs, 6'BL, 15' B-IAPL	0.21
Broad St	Lane Reconfiguration	W->E: 6' BL, 12' TL, 12' CTL, 12' TL, 6' BL	0.06
Riverside Pkwy	Lane Reconfiguration	N->S: 6' BL, 2 12' TLs, 6' BL	0.43
8th Ave	Lane Reconfiguration	N->S: 8' PL, 6' BL, 2 12' TLs, 6' BL	0.09
Turner McCall Blvd	Lane Reconfiguration	N->S: 6' BL, 12' TL, 12' CTL, 12' TL, 6' BL, 12' RTL	0.06
MLK Jr Blvd	Lane Reconfiguration	N->S: 12' RTL, 6'BL, 12' TL, 12' CTL/M, 12' TL, 6' TL	0.06
Harvey St / Calhoun Ave	Lane Reconfiguration	N->S/W->E: 6' BL, 4 12' TLs, 6' BL	0.19
1st Ave	Lane Reconfiguration	N->S: 6' BL, 12' TL, 12' TL, 6' BL	0.08
6th Ave	Requires further study	Requires further study	0.47
Riverside Pkwy	Lane Reconfiguration	N->S: 9' PL, 13' TL, 13' CTL, 13' TL, 6' BL	0.09
1st St	Lane Reconfiguration	SW->NE: 9' PL, 5' BL, 11' TL, 11' CTL, 11' TL, 5'BL	0.12
Glen Milner Blvd Rail-to-Trail Conversion	Lane Reconfiguration	SW->NE: 8' PL, 5' BL, 11' TL, 11' TL, 5'BL	0.08
Glen Milner Blvd Rail-to-Trail Conversion	Lane Reconfiguration	SW->NE: 5' BL, 2 11' TLs, 5' BL	0.08
Glenn Milner Blvd	Lane Reconfiguration	W->E: 10' 2-way CT, 4'Buff, 2 11' TLs	0.45
1st Ave	Lane Reconfiguration	NW->SE: 10' 2-way CT, 4' Buf, 2 12' TLs	0.08
6th St	Lane Reconfiguration	W->E: 7' CT, 5' Buf, 2 12' TLs, 10' M, 2 12' TLs, 5' Buf, 7' CT	0.4
Maple St	Lane Reconfiguration	N->S: 6' BL, 4 10.5' TLs, 6' BL	0.17
Railroad Tracks	Bicycle Boulevard	2 14' TLs with SLMs	0.08
12th St	Lane Reconfiguration	W->E: 5' BL, 2 11' TLs, 5'BL	0.05
2nd Ave	Lane Reconfiguration	N->S: 5' BL, 10.5' TL, 10.5' TL w/SLM	0.63
2nd Ave SE	Lane Reconfiguration	N->S: 6' BL, 4' Buf, 14' TL, 7' M, 14' TL, 4' Buf, 6'BL	0.19

**Proposed Cross Section Abbreviations:** BL= Bike Lane, Buf = Painted Buffer, CT = Cycle Track, SLM = Shared Lane Markings, TL = Travel Lane, CTL = Center Turn Lane, M = Median, B-IAPL= Back-In Angle Parking Lane, RTL = Right Turn Lane Note that all buffers associated with cycle tracks are intended to contain a physical protection element such as planter boxes, a raised curb, or flexible bollards.

## On-street Bikeway Types

Several different kinds of bikeways are recommended in this chapter. Brief descriptions are provided here. Consistent with bicycle facility classifications throughout the nation, these Bicycle Facility Design Guidelines identify the following classes of facilities by degree of separation from motor vehicle traffic.

Note that the “regional bicycle routes” are not a bikeway facility type. These routes appear on selected maps in this plan because they have been previously identified as suitable roads for bicycling despite having no special accommodations for bicyclists. In some cases, these roadways are marked with “Bike Route” or “Share the Road” signage.



### Bike Route

Bike routes along rural roadways can be signed along major recreational and bike touring routes. For roadways with higher speeds and volumes, wide shoulders can create space for bicyclists to ride outside of a travel lane.



### Bike Boulevard

A bike boulevard is a low-volume and low-speed street that has been optimized for bicycle travel through a combination of speed and volume management strategies, wayfinding signage, shared-lane markings, and major-minor intersection crossing treatments.



**Bike Lane**

A bike lane is a portion of the roadway that has been designated by striping, signing, and marking for the preferential and exclusive use of bicyclists.



**Buffered Bike Lane**

Buffered bike lanes are conventional bicycle lanes paired with a designated buffer space, separating the bicycle lane from the adjacent motor vehicle travel lane and/or parking lane.



**Cycle Track**

Also known as protected bike lanes, cycle tracks provide physical separation from motor vehicle traffic through the use of a raised curb, flexible bollards, trees, parked cars, planter boxes, or other elements.

## Bikeway Implementation Strategies

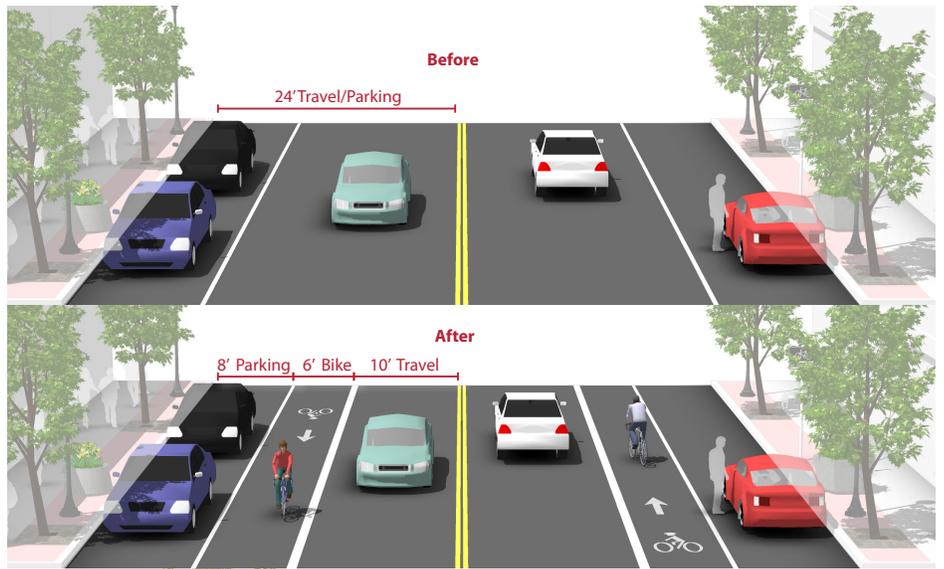
Most major streets are characterized by conditions (e.g., high vehicle speeds and/or volumes) for which dedicated bike lanes are the most appropriate facility to accommodate safe and comfortable riding. Although opportunities to add bike lanes through roadway widening may exist in some locations, many major streets have physical and other constraints that would require street retrofit measures within existing curb-to-curb widths.

As a result, much of the guidance provided in this section focuses on effectively reallocating existing street width through striping modifications to accommodate dedicated bike lanes. Or in the case of bike boulevards, design streets to be low volume, low stress bike routes through a neighborhood.

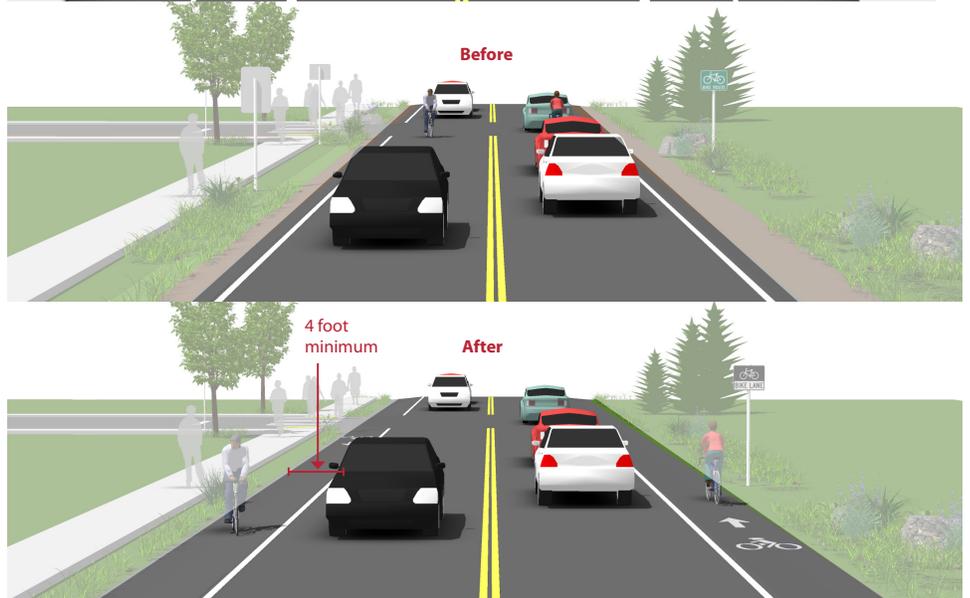
These types of strategies can be implemented during regularly scheduled repaving or maintenance projects, done during streetscape enhancement projects, or done as stand-alone re-striping projects. For more discussion on these types of implementation strategies, see the Implementation chapter of this Plan.



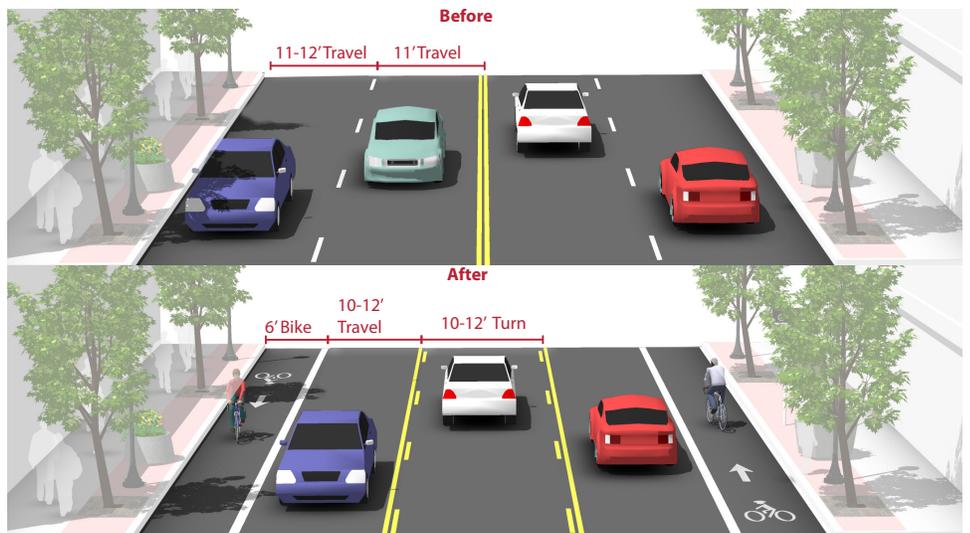
**Lane Narrowing**



**Lane Expansion**



**Lane Reconfiguration**



## Support Facilities

Creating a biking culture in Rome-Floyd County and increasing the convenience to bike will require more than expanding the network. Support infrastructure such as bike parking and wayfinding can improve and expand how one gets around by bike, how one navigates the community, and how someone secures their bike at the end of their trip.

### Bike Parking

**Bicyclists expect a safe, convenient place to secure their bicycle when they reach their destination. This may be short-term parking of two hours or less or long-term parking for employees, students, residents, and commuters.**



#### Bike Racks

Short-term bicycle parking is meant to accommodate visitors, customers, and others expected to depart within two hours. It should have an approved standard rack, appropriate location and placement, and weather protection. The Association for Pedestrian and Bicycle Professionals (APBP) recommends selecting a bicycle rack that:

- Supports the bicycle in at least two places, preventing it from falling over
- Allows locking of the frame and one or both wheels with a U-lock
- Is securely anchored to ground
- Resists cutting, rusting, and bending or deformation

#### Secure Bike Parking



#### Bike Corrals

Bike corrals (also known as on-street bicycle parking) consist of bicycle racks grouped together in a common area within the street or in a parking lot traditionally used for automobile parking. Bike corrals are reserved exclusively for bicycle parking and provide a relatively inexpensive solution to providing high-volume bicycle parking. Bike corrals can be implemented by converting one or two motor vehicle parking spaces into on-street bicycle parking. Each motor vehicle parking space can be replaced with approximately 6-10 bicycle parking spaces.

A Secure Parking Area for bicycles, also known as a Bike SPA or Bike & Ride (when located at transit stations), is a semi-enclosed space that offers a higher level of security than ordinary bike racks. Accessible via key-card, combination locks, or keys, Bike SPAs provide high-capacity parking for 10 to 100 or more bicycles. Increased security measures create an additional transportation option for those whose biggest concern is theft and vulnerability. Bike SPAs can be standalone structures or occupy space within a building.



*Wayfinding signs help bicyclists choose the best streets for biking to popular destinations.*

## Wayfinding

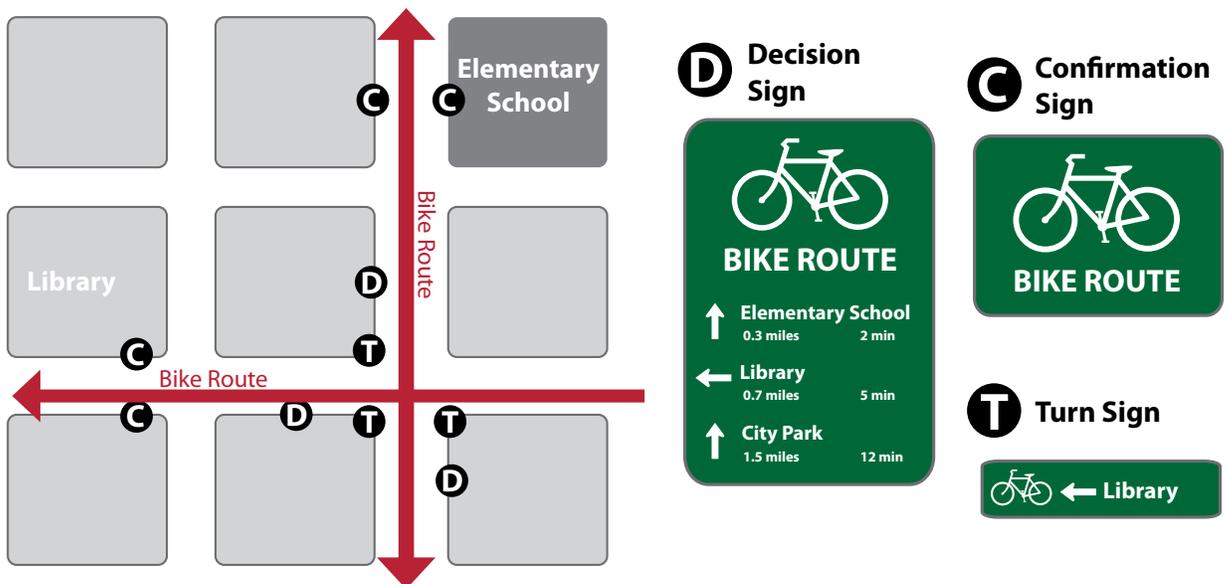
Wayfinding signage, as part of a signage program that also includes warning and regulatory signage, enhances resident and visitor orientation. A clear wayfinding system should contribute to economic development by pointing visitors to key destinations around the City. Rome-Floyd should develop a customized wayfinding program that includes directional signage to local destinations. Bicycle and pedestrian travel times to popular destinations could also be included on directional signage.

Bicycle wayfinding signs also visually cue motorists that they are driving along a bicycle route and should use caution. Signs are typically placed at key locations leading to and along bicycle routes, including at the intersection of multiple routes. Too many road signs tend to clutter the right-of-way, and it is recommended that these signs be posted at a level most visible to bicyclists rather than per vehicle signage standards.

### Wayfinding Sign Types and Placement

Signs are typically placed at decision points along bicycle routes – typically at the intersection of two or more bikeways and at other key locations leading to and along bicycle routes.

- **Confirmation Signs.** Every ¼ to ½ mile along off-street facilities and every 2 to 3 blocks along on-street bicycle facilities, unless another type of sign is used (e.g., within 150 ft of a turn or decision sign). Should be placed soon after turns to confirm destination(s). Pavement markings can also act as confirmation that a bicyclist is on a preferred route.
- **Turn Signs.** Near-side of intersections where bike routes turn (e.g., where the street ceases to be a bicycle route or does not go through). Pavement markings can also indicate the need to turn to the bicyclist.
- **Decision Signs.** Near-side of intersections in advance of a junction with another bicycle route. Along a route to indicate a nearby destination.



# Program Recommendations

## Overview

Of the Five E's of a Bicycle Friendly or Walk Friendly Community, four are related to programs: education, encouragement, enforcement, and evaluation. Programs will compliment engineering improvements, such as bike lanes and trails, and give the Rome-Floyd County community the tools they need to safely and confidently walk and bike. Additionally, walking and biking related policies impact all Five E's and serve as evaluation and planning tools for institutionalizing the principles of bicycling and walking friendliness.

All of the Five E's work together to enhance the biking and waking experience in Rome-Floyd County. The following sections present recommended programs to support the vision and goals of this Plan. The recommendations can be undertaken by local and regional agencies, community organizations, local businesses, or partnerships among these groups.

## Education

### Safe Routes to School

Safe Routes to School (SRTS) is a national program that supports improving walking and biking access to schools as well as programs to educate students about how to walk and bike safely to school. It can be a resource for grant funds to build improvements as well as information resource for education and encouragement programs at schools.

The Rome Planning Department, in partnership with the Rome and Floyd County School Districts, has conducted several SRTS projects. The Rome-Floyd County MPO should continue to regularly meet with local school representatives to coordinate walking and biking projects around schools. Additionally, events, such as bike rodeos, should be coordinated with schools.

#### Additional Resources:

- **National Center for Safe Routes to School:** <http://www.saferoutesinfo.org/>



*Road safety education can help overcome barriers to walking or biking to school.*



*Hands-on courses help students develop the skills needed to bike on the road.*

**SAFETY DOESN'T HAPPEN BY ACCIDENT.**

**ALERT TODAY ALIVE TOMORROW**

[www.AlertTodayFlorida.com](http://www.AlertTodayFlorida.com)

**WHEN DRIVING, WALKING, OR BICYCLING... PAY ATTENTION. READ THE SIGNS. LEARN THE RULES.**

**AVOID DISTRACTIONS.**

**STOP BEFORE TURNING RIGHT ON RED.**

**USE THE SIDEWALK AND CROSSWALKS.**

**BICYCLE PREDICTABLY, WITH TRAFFIC.**

Funded by the Florida Department of Transportation

**FDOT** **CUTR**

BICYCLIST TIPS	PEDESTRIAN TIPS
Ride in the same direction of traffic on the street. Obey all traffic signs, signals, and lane markings.	Obey all traffic laws. If no sidewalks are present, walk against the direction of traffic so you can see oncoming vehicles.
Yield to traffic when appropriate. It's better to yield than "be in the right" and risk injury.	Always use the crosswalk. Push the button to get the walk signal.
Be visible. Position yourself in the lane so others can easily see you. Always use a headlight and taillight at night.	Be visible. Wear bright, reflective colors on your clothes, shoes, belts and wristbands.
Watch for parked cars. Don't ride in the "door zone" of parked cars even when the bike lane is adjacent to them.	Look, then look again. Look left, right, then left again before crossing.

[www.AlertTodayFlorida.com](http://www.AlertTodayFlorida.com)

*The Florida Department of Transportation's "Alert Today, Alive Tomorrow" campaign includes messages for drivers, pedestrians, and bicyclists.*

## Bike Rodeos and Classes

A Bike Rodeo is an event where children can learn and practice bicycling skills in a controlled, supervised environment. Depending on the age of the children involved, a bike rodeo event can include educational components, such as teaching hand signals, proper helmet fitting, and even basic maintenance skills such as changing and inflating a tire. The highlight of any bike rodeo event is a skills course, where children ride through a designed obstacle course to practice turns, braking, and coasting. Some bike rodeo leaders hand out awards to positively reinforce good bicycling habits.

### Additional Resources:

- **National Center for Safe Routes to School:** <http://www.saferoutesinfo.org/program-tools/organizers-guide-bicycle-rodeos>
- **Safe Kids Worldwide:** <http://www.safekids.org/sites/default/files/documents/Bike-Rodeo-Station-Guide.pdf>

## Safety and Trail User Etiquette Campaigns

Educational campaigns that target motorists and those walking, biking, and taking transit creates a shared sense of responsibility among all roadway users, rather than singling out one user group. A joint campaign can focus on rights and responsibilities of everyone using the streets and trails of Rome, Floyd County, and Cave Spring. Information may include important bicycle laws, tips for safe bicycle travel, safe street crossing actions, keys to safe motor vehicle operation, and general facility rules and regulations.

Educational materials are often available for download from national organizations, such as the Pedestrian and Bicycle Information Center, and state or local resources, such as GDOT.

This Plan recommends that Bike! Walk! Northwest Georgia (BWNWGA) and the Rome-Floyd County MPO establish a variety of outreach strategies for educating motorists and those walking and biking about safely sharing the street. BWNWGA should also focus on trail user etiquette to promote safe trail behavior. Campaign activities can include distributing materials at local events or print advertisements and public service announcements.

### Additional Resources:

- **Pedestrian and Bicycle Information Center – Programs and Campaigns:** <http://www.pedbikeinfo.org/programs/index.cfm>
- **Atlanta BeltLine Etiquette Campaign:** <http://beltline.org/visit/atlanta-beltline-etiquette/>

## Encouragement

### Open Street Event

Car-free, open street events have many names - Sunday Parkways, Ciclovias, Summer Streets, and Sunday Streets - and involve periodic street “openings” that create a temporary park that is open to the public for walking, bicycling, dancing, and other physical activity. The purpose of the event is to encourage physical activity by providing a fun, welcoming environment for activity. Car-free street events have been very successful internationally and are rapidly becoming popular in the U.S. Local businesses open doors and set up tables along sidewalks to support the event and generate foot and bike traffic for their businesses.

#### Additional Information:

- **Open Streets Project:** <http://openstreetsproject.org/>
- **Gables Bike Day:** <http://openstreetsproject.org/coralgables/>

### Silver Sneaker Program

The City, County, Health Department, or local health care providers can develop a Silver Sneaker Program with a focus on walking and biking that is specifically tailored for older adults. Activities could include adult tricycle or bicycle rides, nature walks, and safety education about walking and biking.

This type of program can be incorporated into existing Parks and Recreation programs or done in partnership with other groups, such as community organizations or assisted living centers.

#### Additional Resources:

- **Silver Sneakers Fitness Program:** <http://silversneakers.com/>



*Open streets events allow people to connect with the people and places in their community in new ways.*



*Silver Sneaker programs provide social and physical activity opportunities for older adults.*



**Duck, NC's bicycle and pedestrian resource web page.**



**Encouragement events should focus on the fun.**



**A bicycle-friendly business program could be particularly attractive to businesses on Broad Street.**

## Bicycle and Pedestrian Resource Webpage

Many current and potential pedestrians and bicyclists do not know where to find information on traffic laws, events, maps, tips, and recreation groups. The Rome-Floyd County government could develop a “one-stop” webpage on the government’s website that houses all pedestrian- and bicycle-related information and promotions. A webpage is not difficult to set up, but it will only be successful if the site is easy to use, easy to find, and updated frequently. The site should be reviewed and updated regularly with the most current information. A Bicycle and Pedestrian Advisory Committee can assist in keeping the site up-to-date. Other recommended programs in this chapter could be housed on the website, such as a bike map, safety campaign information, and a calendar of upcoming events.

### Additional Resources:

- **Portland, OR:** <http://www.portlandoregon.gov/transportation/60164>
- **Duck, NC:** <http://www.townofduck.com/ducktrail/>

## National Bike Month Events

Cities and towns across the country participate in National Bike Month annually, which is held in May. The League of American Bicyclists hosts a website for event organizers.

It is recommended that Rome-Floyd County host National Bike Month events and activities annually, with support from the Bicycle and Pedestrian Advisory Committee and local bicycling groups and businesses. Examples of Bike Month activities include:

- A bike ride with the Mayor
- Bike to Work Day events, such as morning commuter energizer stations with food encouragement, information, and sponsored goodies for participants
- Bike Commuter Challenge program with awards for participants

### Additional Resources:

- **National Bike Month:** <http://bikeleague.org/bikemonth>

## Employer-Based Programs

A Bicycle and Pedestrian Advisory Committee, local community organizations, Chamber of Commerce, or the City and County should encourage local businesses to offer discounts to patrons who bicycle or walk to their business. The discount could be a daily or once-weekly promotion that encourages residents and visitors to visit local businesses by a mode other than motor vehicle. This could be especially popular with businesses in areas that have limited motor vehicle parking and overly congested lots. In return for businesses’ participation, the City, County, or Chamber of Commerce could develop a list of “Bicycle-Friendly Local Businesses” to feature, along with discount information, on its one-stop website for bicycle and pedestrian information.

## Hike and Bike Map

One of the most effective ways of encouraging people to walk and bicycle is through the use of maps and guides to show where you can walk and bike, and to guide people to enjoyable routes and destinations for walking and biking. The Rome-Floyd County should create a community Hike and Bike Map to reflect the most current public bicycle and pedestrian infrastructure in town, with a list of bicycle rental locations, suggestions for self-guided bike rides and walks around town, and recommended routes. A portion of the map could be devoted to bicycle and pedestrian safety education, such as informational graphics that demonstrate bicycle hand signals and how to share the road and the trail safely. The map should be made available online and printed as needed to be actively distributed to residents and visitors. It should also be updated on a regular basis as new facilities are implemented.

### Sample Maps and Self-Guided Walks:

- <http://www.bikewalktwincities.org/maps-routes/walking-maps>
- <http://durhamnc.gov/ich/op/dot/Pages/Durham-Bike--Hike-Map.aspx>

## Wayfinding Program

Wayfinding signage, as part of a signage program that also includes warning and regulatory signage, enhances resident and visitor orientation. A clear wayfinding system should contribute to economic development by pointing visitors to key destinations around town. Rome-Floyd County should develop a customized wayfinding program that includes directional signage to local destinations. Bicycle and pedestrian travel times to popular destinations could also be included on directional signage.

Materials for signage should reflect the character of Rome-Floyd County and be selected for longevity and ease of maintenance. A wayfinding program could include directional signage, on-road markings, and kiosks with town maps. If funding is not immediately available to develop a complete wayfinding program, a good first step is temporary wayfinding signage that is colorful and informative. The Chamber of Commerce, local bicycle clubs, or local businesses may be ideal partners based on the nexus with tourism and economic development.

### Sample wayfinding signage programs:

- *Oakland, CA:* <http://www2.oaklandnet.com/oakca/groups/pwa/documents/report/oak025118.pdf>
- *2014 Croatan Regional Bicycle + Trails Plan; Signage Appendix (NCDOT)*



*Durham, NC's Hike and Bike Map helps residents and visitors navigate the city on foot or on two wheels.*



*Oakland, CA's bicycle wayfinding design guidelines include detailed graphics of each type of wayfinding sign.*



*Police officers with strong knowledge of bicycle and pedestrian laws are an asset to the community.*



*Targeted enforcement actions can have positive impacts on behavior.*

## Enforcement

### Safety Training for Officers

Public safety officer training courses provide officers with safety education related to the rights and responsibilities of bicyclists, pedestrians, and motorists. The training can explain matters such as common errors in reporting a bicycle or pedestrian collision, laws related to motorists passing a bicyclist, or pedestrians and crosswalk use.

#### Additional Resources

- ***Pedestrian and Bicycle Information Center – Law Enforcement:*** [http://www.pedbikeinfo.org/programs/enforcement\\_enforcelaws.cfm](http://www.pedbikeinfo.org/programs/enforcement_enforcelaws.cfm)

### Targeted Enforcement

Public safety officers should conduct regularly scheduled enforcement efforts to educate and enforce laws that impact walking and biking. The enforcement should be conducted in areas where there are known safety issues, such as high bicycle or pedestrian crash locations. These efforts should support an active and safe walking and biking culture in the community.

### Public Safety Officer on BPAC

To re-enforce the focus on enforcement and to help with coordination, a public safety officer should serve on the Bicycle and Pedestrian Advisory Committee (BPAC) when it is formed. The officer can provide regular updates on bicycle and pedestrian crashes, coordinate safety campaigns with the BPAC, and help with other safety related policies that help implement this Plan.

## Evaluation/Planning

### Conduct Annual Bike/Ped Counts at Key Locations

The BPAC, in coordination with Rome-Floyd County and regional partners, such as the Northwest Georgia Regional Commission, should establish an annual bicycle and pedestrian count and survey program. The effort will help document increases in walking and biking rates, measure impacts of facilities after they are installed, and document and track needs and concerns of those currently walking and biking in Rome-Floyd County. The count program can include manual counts and surveys, online surveys, or installing automated counters along key routes, such as the Heritage Park Trail.

#### Additional Resources:

- **National Bicycle and Pedestrian Documentation Project:** <http://bikepeddocumentation.org/>
- **Pedestrian and Bicycle Information Center – Data Collection Tools:** <http://www.pedbikeinfo.org/planning/tools.cfm>

### Conduction of Roadway Safety Audits

Safety audits help identify safety issues related to walking and biking along a specific roadway as well as potential solutions to correct safety issues. Audits typically include analyzing available data, conducting site visits with multiple stakeholders, and forming consensus on action steps to improve safety conditions along the street.

#### Additional Resources

- **Pedestrian and Bicycle Information Center:** [http://www.pedbikeinfo.org/planning/tools\\_audits.cfm](http://www.pedbikeinfo.org/planning/tools_audits.cfm)
- **FHWA Road Safety Audit:** <http://safety.fhwa.dot.gov/rsa/>



*Better data on walking and biking can assist with future planning efforts.*



*Road safety audits can help decision makers prioritize safety issues along corridors.*

## Policy Recommendations

Policy recommendations are based on a review of existing codes, ordinances, and land use regulations for Rome and Floyd County. While many elements of the current policies are supportive of bicycle and pedestrian friendly development, it is evident that the City and County could significantly strengthen several areas of policy regarding complete streets, bicycle parking standards, and walking and biking infrastructure. The following sections provide recommended “next steps” for improving the bicycle and walk-friendliness of local policies.

### Complete Streets Policy

A Complete Street is a roadway that, in addition to general purpose vehicular travel lanes, includes items such as sidewalks, bike lanes or shoulders, bus lanes, transit stops, crosswalks, median refuges, curb bulb-outs, appropriate landscaping, and other features that add to the usability and livability of the street as determined by context.

This Plan recommends that Rome, Floyd County, and Cave Spring adopt a Complete Streets policy. In addition to adopting a Complete Streets policy, the Cities and County should develop and adopt street design guidelines to support the policy and communicate desired street treatments.

According to the National Complete Streets Coalition, an ideal Complete Streets Policy should include the following elements:

- Includes a vision for how and why the community wants to complete its streets.
- Specifies that “all users” includes pedestrians, bicyclists, and transit users of all ages and abilities, as well as trucks, buses, and automobiles.
- Applies to both new and retrofit projects, including design, planning, maintenance, and operations, for the entire right-of-way.
- Makes any exceptions specific and sets a clear procedure that requires high-level approval of exceptions.
- Encourages street connectivity and aims to create a comprehensive, integrated, and connected network for all modes.
- Is adoptable by all agencies or departments to cover all roads.
- Directs the use of the latest and best design criteria and design guidelines while recognizing the need for flexibility in balancing user needs.
- Directs that complete streets solutions will complement the context of the community.

#### Additional Resources:

- **GDOT Complete Streets Policy:** <http://www.dot.ga.gov/travelingingeorgia/completestreets/Pages/default.aspx>
- **GDOT Design Policy Manual:** <http://www.dot.ga.gov/doingbusiness/PoliciesManuals/roads/designpolicies/Pages/DesignPolicyManual.aspx>
- **National Complete Streets Coalition:** <http://www.smartgrowthamerica.org/complete-streets/changing-policy>

## Form a Bicycle and Pedestrian Advisory Committee

The Rome-Floyd County MPO should form a Bicycle and Pedestrian Advisory Committee (BPAC) to assist in the implementation of this Plan. The BPAC should have representation from active pedestrians and commuting and recreational cyclists and should champion the recommendations of this Plan. The formation of this group would be a significant step in becoming designated as a Bicycle Friendly and Walk Friendly Community. The committee would provide a communications link between the citizens of the community and local government. They should also continue to meet periodically and be tasked with assisting City and County staff in community outreach, marketing, and educational activities recommended by this Plan. The BPAC should include representation from community groups and businesses, local agencies and departments, and regional agencies and departments.

## New Development

When re-zoning applications or site plans are submitted to the City of Rome or Floyd County, they should be reviewed for concurrency with this Plan. Street improvements, such as providing right-of-way for sidewalks, bike lanes, or trails or bicycle and pedestrian access to a site, should be assessed. Additionally, site designs for new developments should address bicycle and pedestrian circulation and access to buildings on the site, such as bicycle parking.



*New development can often provide an opportunity to enhance streets as well as access to buildings.*

**CHAPTER OUTLINE:**

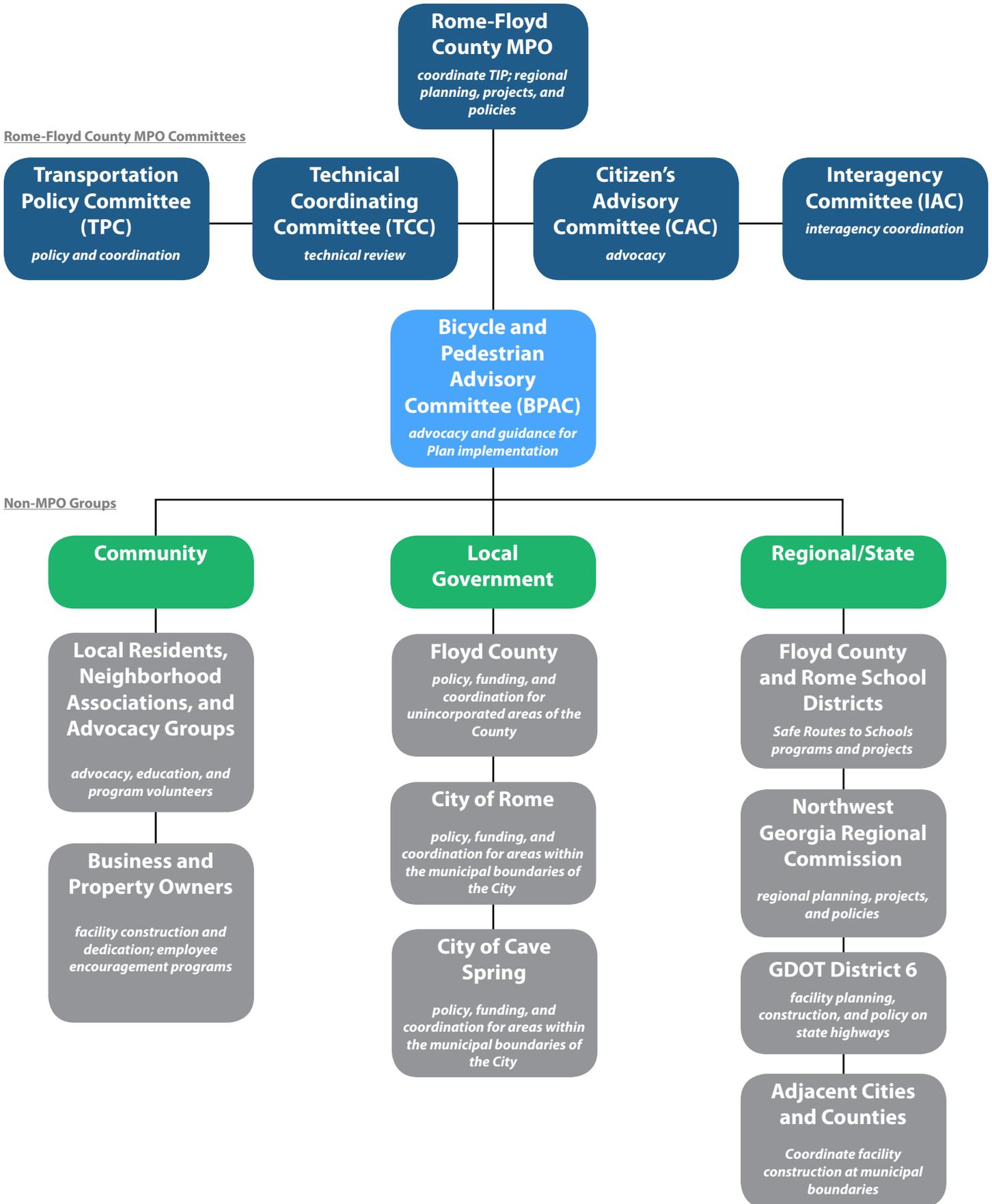
- Overview**
- Roles for Implementation**
- Funding Strategy**
- Key Action Steps**
- Infrastructure Implementation Strategies**

**Overview**

This chapter defines a structure for managing the implementation of the Rome-Floyd County MPO Bicycle, Pedestrian, and Trail Master Plan. Implementing the recommendations within this plan will require leadership and dedication to bicycle and pedestrian facility development on the part of a variety of groups and agencies. Equally critical, and perhaps more challenging, will be meeting the need for a recurring source of revenue. Even small amounts of local funding could be very useful and beneficial when matched with outside sources. Most importantly, the MPO and its local partners need not accomplish the recommendations of this Plan by acting alone; success will be realized through collaboration with regional and state agencies, the private sector, and non-profit organizations.

Given the constant change in funding availability at local, state, and federal levels, it is difficult to know what financial resources will be available at different time frames during the implementation of this Plan. However, there are still important actions to take in advance of major investments, including key organizational steps, the initiation of education and safety programs, and the development of strategic, lower-cost infrastructure improvements. Following through on these priorities will allow the key stakeholders to prepare for the development of larger walkway or bikeway projects over time, while taking advantage of strategic opportunities as they arise.

# Roles for Implementation

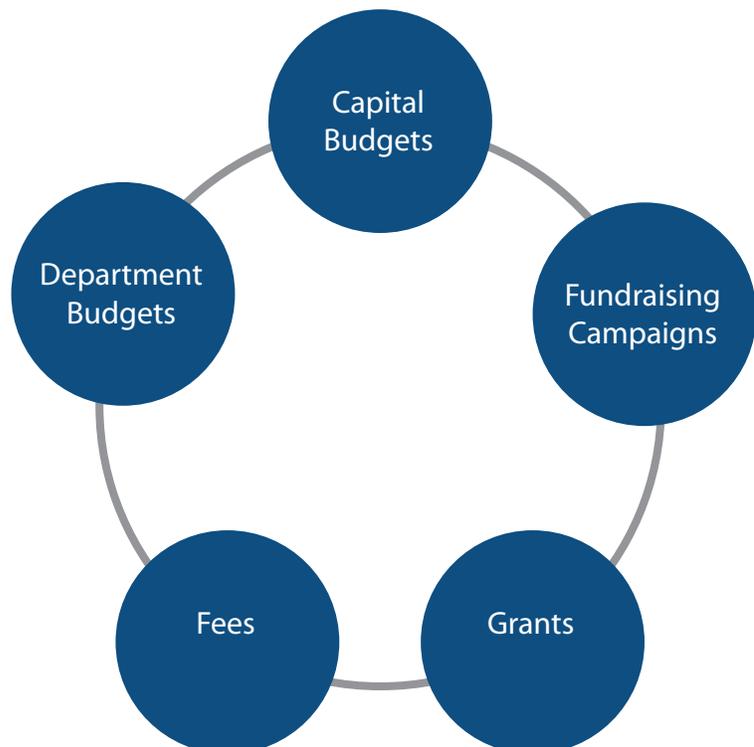


## Funding Strategy

In order to achieve the goals of this Plan, the Rome-Floyd County MPO and its partners will need to fund improvements from a variety of funding sources and partners. Rome-Floyd County will need to be opportunistic and consistent in sourcing funding for this plan. Particularly when dealing with federal funding for transportation projects, the planning and construction process can take years.

Five primary sources of funding make up the core funding strategy for this Plan:

- **Capital Budgets** - The MPO can use the concepts and policies presented in this Plan to implement this Plan through regularly scheduled capital projects, such as streetscape projects, street resurfacing, or new public or private property construction.
- **Department Budgets** - Departments like Public Works or Parks and Recreation can use their maintenance resources and staff to support programs and infrastructure maintenance.
- **Fees** - User fees or development impact fees provide an opportunity to generate revenue to fund infrastructure projects, such as sidewalk construction, and programs, such as bicycle education classes.
- **Grants** - Competitive grants through public agencies or through private or non-profit foundations can generate additional resources for projects and programs.
- **Fundraising Campaigns** - Fundraising through neighborhood groups, advocacy groups, or even crowd-funding can help generate additional resources for projects and programs.



## Key Action Steps

Task	Lead Agency	Support
<b>Policy Action Steps</b>		
Adopt this Plan	Rome-Floyd County MPO	Member governments and agencies
Coordinate Development Plans	Rome-Floyd County Planning Department	Rome-Floyd County Infrastructure Departments
Form a Bicycle and Pedestrian Advisory Committee	Rome-Floyd County MPO	Member governments and agencies; Community advocacy organizations
Seek Multiple Funding Sources and Facility Development Options	Rome-Floyd County MPO	BPAC; Community, Local Government, and Regional/State partners
Adopt a Complete Streets Policy and Guidelines	Rome-Floyd County MPO; BPAC	Board of Commissioners
<b>Program Action Steps</b>		
Designate Staff	City and County Board of Commissioners	Rome-Floyd County MPO; BPAC
Become designated as a Bicycle Friendly Community (BFC)	BPAC	Rome-Floyd County MPO; City of Rome; City of Cave Spring; Floyd County
Become designated as a Walk Friendly Community (WFC)	BPAC	Rome-Floyd County MPO; City of Rome; City of Cave Spring; Floyd County
Communication and Outreach	BPAC; local advocacy groups; local bike shops and cycling clubs	Rome-Floyd County MPO
Establish Evaluation and Reporting Program	Rome-Floyd County MPO; Planning Department	BPAC
Begin Annual Meeting with Key Project Partners	Rome-Floyd County MPO; BPAC	Community, Local Government and Regional/State partners
Improve Existing Programs and Launch New Programs	BPAC	Community and Local Government partners
Provide Enforcement and Education Training for Public Safety Officials	Rome and Floyd County Police; Floyd County Sheriffs; Fire Department	BPAC
<b>Infrastructure Action Steps</b>		
Identify Funding	Rome-Floyd County MPO	Board of Commissioners; BPAC
Complete Short-Term Priority Projects	Rome-Floyd County MPO; BPAC	Planning Department; City and County Engineering

Details	Phase
Through adoption, the Plan becomes an official planning document of the MPO. Adoption shows that the MPO has undergone a successful, supported planning process.	Short-Term (2015)
During the development review process, City and County staff should reference this Plan. If a new development requires changing the public right-of-way, the changes should be used to support walking and biking improvements identified in this Plan. The site design should also be supportive of walking and biking access on the property.	Ongoing
Form the Bicycle and Pedestrian Advisory Committee and confirm the goals of the BPAC to include the implementation of this Plan.	Short-Term (2015)
To implement this Plan, funding from a variety of funding sources will need to be leveraged. Working with MPO and other partners, the BPAC should identify public and private funding sources and pursue these resources on an ongoing basis.	Short-Term/Ongoing (2015 onward)
The BPAC and City and County staff should work in partnership with the Board of Commissioners to adopt a Complete Streets Policy.	Short- to Mid-Term (2015-2016)
Designate staff to oversee the implementation of this Plan and the proper maintenance of the facilities that are developed. Designated staff should include City and County staff.	Short-Term (2015)
The development and implementation of this Plan is an essential first step toward becoming a designated BFC. With ongoing efforts and the short- term work program recommended here, the community should be in a position to apply for and receive recognition within a few years.	Mid-Term (2017)
The development and implementation of this Plan is an essential first step toward becoming a designated WFC. With ongoing efforts and the short- term work program recommended here, the community should be in a position to apply for and receive recognition within a few years.	Mid-Term (2017)
The BPAC should establish a communication campaign to celebrate successes as facilities are developed and otherwise raise awareness of the overall pedestrian and bicycle network and its benefits. A key first task of this group is to design and launch a one-stop website. Set up the one-stop website to provide information to residents and tourists on walking in Rome-Floyd County. To begin, the website can include the maps included in this Plan.	Short-Term (2015)
The MPO, Planning Department, and the BPAC should brainstorm specific benchmarks to track through a monitoring program and honor the completion of projects with public events and media coverage.	Mid-Term/Ongoing (2016 onward)
Key project partners (see org. chart on page 66) should meet on an annual basis to evaluate the implementation of this Plan. Meetings could also occasionally include on-site tours of priority project corridors.	Short-Term/Ongoing (2015 onward)
These groups should coordinate to improve existing bicycle and pedestrian programs and to launch new programs, such as those described in Chapter 3.	Short-Term/Ongoing (2015 onward)
Provide police and fire officers with training through free online resources available from the National Highway Traffic Safety Administration and through webinars available through the Association of Pedestrian and Bicycle Professionals. Provide officers with an informational handout to be used during bicycle and pedestrian-related citations and warnings. Coordinate regular in-person training workshops for officers to learn bicycle and pedestrian laws and enforcement strategies.	Short-Term/Ongoing (2015/2016 onward)
To allow continued development of the overall walkway and bikeway system, capital funds for pedestrian and bicycle facility construction should be set aside every year. Local and federal funds should be programmed for facility construction. Funding for an ongoing maintenance program should also be included in the Cities' and County's operating budgets.	Short-Term/Ongoing (2015 onward)
The Recommendations chapter provides a list of projects. Aim to complete at least two of these projects by the end of 2017.	Mid-Term (2017)

## Policy Action Steps

Several policy steps are crucial to the success of future facility development. These steps will legitimize the recommendations found in this Plan and support policy decision-making necessary to carry out those recommendations.

### **Adopt This Plan**

Before any other action takes place, the MPO should adopt this Plan. This should be considered the first step in implementation. Through adoption of this Plan, the MPO will be better able to shape transportation and development decisions so that they fit with the goals of this Plan and the Rome-Floyd County Long Range Transportation Plan. Most importantly, having an adopted plan is extremely helpful in securing funding from state, federal, and private agencies. Adopting this Plan does not commit the MPO to dedicate or allocate funds, but rather indicates intent to implement this Plan over time.

### **Coordinate Development Plans**

The MPO and Planning Department should ensure that adopted walkways, bikeways, trails, and walking and biking support infrastructure recommendations from this Plan are included in future residential and commercial developments that connect with such proposed facilities. Changing development regulations to include requirements for sidewalks, bike parking, and providing easements for future trail development should also be reviewed.

### **Form a Bicycle and Pedestrian Advisory Committee**

The MPO should form a Bicycle and Pedestrian Advisory Committee (BPAC) to assist in the implementation of this Plan. The BPAC should have representation from active pedestrians and commuting and recreational cyclists, local government representatives, and other groups with an interest in improving walking and biking in Rome-Floyd County. The committee should champion the recommendations of this Plan and the formation of this group would be a significant step in becoming designated as a Walk Friendly Community or Bicycle Friendly Community. The committee would provide a communications link between the citizens of the community and local government. They should also continue to meet periodically and be tasked with assisting the MPO staff in community outreach, marketing, and educational activities recommended by this Plan.

## **Seek Multiple Funding Sources and Facility Development Options**

Multiple approaches should be taken to support bicycle and pedestrian facility development and programming. It is important to secure the funding necessary to undertake priority projects but also to develop a long-term funding strategy to allow continued development of the overall system. Dedicated local funding sources will be important for the implementation of this Plan. Capital and local funds for pedestrian facilities and trail construction should be set aside every year, even if only for a small amount. Small amounts of local funding can be matched to outside funding sources or could be used to enhance federally-funded projects with pedestrian features that may otherwise not be budgeted for by the state. A variety of local, state, and federal options and sources exist and should be pursued.

A priority action is to immediately evaluate the recommendations against transportation projects that are currently programmed in the Rome-Floyd County MPO Transportation Improvement Program (TIP) to see where projects overlap, compliment, or conflict with each other. The MPO should also evaluate which of the proposed projects could be added to future TIP updates and which projects can be handled entirely by local money or private funding.

## **Adopt a Complete Streets Policy and Guidelines**

The Cities of Rome and Cave Spring and Floyd County should work with the Rome-Floyd County MPO to adopted a Complete Streets Policy. The group can use resources from GDOT and the National Complete Streets Coalition (they have sample policies from around the country to draw from) to develop and tailor a Policy consistent with the community's context and goals. The Policy itself need not be cumbersome in its language; however, the key implementation component associated with the Policy is the subsequent development of design guidelines, such as typical cross sections, that can be applied in varied contexts throughout each of the member jurisdictions.

## Program Action Steps

While policies provide a legal basis for on- and off-road facility development, the program recommendations included in the Recommendations chapter of this Plan will build community support for the creation of new facilities and establish a strong bicycling and walking culture.

### Designate Staff

Designate staff to oversee the implementation of this Plan and the proper maintenance of the facilities that are developed. It is recommended that a combination of existing Planning, Public Works, and City and County management staff oversee the day-to-day implementation of this Plan. In many municipalities, a full-time bicycle and pedestrian coordinator covers this task, but in smaller communities, such as Rome-Floyd County, it makes more sense to fold these responsibilities into current staff responsibilities.

### Become Designated as a Bicycle Friendly and Walk Friendly Community

A goal for Rome, Floyd County, and Cave Spring should be to seek a Walk Friendly Community (WFC) designation from the UNC Highway Safety Research Center's Pedestrian and Bicycle Information Center and Bicycle Friendly Community (BFC) designation from the League of American Bicyclists. The WFC and BFC campaigns are awards programs that recognize municipalities that actively support pedestrian and bicycle activity and safety. A WFC and BFC provides safe accommodation for walking and biking and encourages its residents to walk and bike for transportation and recreation.

Becoming designated as a WFC and/or a BFC signals to current residents, potential residents, and visitors that the town is a safe and welcoming place for individuals and families to live and recreate. The development and implementation of this Plan is an essential first step toward becoming a WFC or BFC. With ongoing efforts and the short-term work program recommended here, the MPO and its member governments should be in a position to apply for and receive WFC and BFC status within a few years.

### Communication and Outreach

The BPAC should lead the effort to establish a communication campaign to celebrate successes as facilities are developed and otherwise raise awareness of the overall bicycle and pedestrian network and its benefits. A key first task of this group is to design and launch a one-stop website or page on the City-County website about walking and biking initiatives, programs, and infrastructure across Floyd County.

Many current and potential pedestrians and bicyclists do not know where to turn to find out about traffic laws, events, maps, tips, and groups. Developing a “Walk and Bike Central” site online provides information to a wide audience and encourages people to walk and bike. A one-stop website is not usually difficult to set up, but it will only be successful if the site is both easy to use and updated frequently. All site content should be reviewed regularly for accuracy. Walking groups, the bicycling community, and volunteer organizations interested in safety and health can assist in keeping the site up-to-date.

### **Establish Evaluation and Reporting Program**

From the beginning, and continuously through the life of this Plan, the BPAC should establish specific benchmarks to track through a monitoring program. The monitoring will help track progress towards achieving the goals of this Plan and honor the implementation successes associated with this Plan, such as completion of projects with public events and media coverage. The BPAC should also develop an annual report to document work towards achieving the goals and recommendations in this Plan.

### **Begin Annual Meeting with Key Project Partners**

Coordination between key project partners will establish a system of checks and balances, provide a level of accountability, and ensure that recommendations are implemented. This meeting should be organized by the designated government staff and should include representatives from the Roles for Implementation chart shown on page 66. The purpose of the meeting should be to ensure that this Plan’s recommendations are integrated with other transportation planning efforts in the region as well as long-range and current land use planning, economic development planning, and environmental planning. Attendees should work together to identify and secure funding necessary to immediately begin the first year’s work, and start working on a funding strategy that will allow the MPO and its government partners to incrementally complete each of the suggested physical improvements, policy changes, and programs over a 5-10 year period. A brief progress benchmark report should be a product of these meetings, and participants should reconfirm the Plan’s goals each year. The meetings could also occasionally feature special training sessions on pedestrian, bicycle, and trail issues.

## **Improve Existing Programs and Launch New Programs**

The program recommendations found in the Recommendations chapter provide a set of programmatic resources that will support the goals of this Plan. The MPO should reference the recommendations to expand and improve upon existing programs as well as to develop new programs that promote walking.

Through cooperation between the MPO, City and County government, the BPAC, schools, and groups such as walking and bicycling clubs, strong education, encouragement, and enforcement campaigns could also occur as new facilities are built. When an improvement has been made, the roadway environment has changed and proper interaction between motorists, bicyclists, and pedestrians is critical for the safety of all users. A campaign through local television, on-site enforcement, education events, and other methods will bring attention to the new facility, and educate, encourage, and enforce proper use and behavior. The Recommendations chapter provides program ideas to choose from, some of which are included in the action steps table at the beginning of this chapter.

## **Provide Enforcement and Education Training for Public Safety Officials**

Public safety officers, such as Police and Fire, have many important responsibilities, including improving safety conditions and behaviors that impact pedestrians and bicyclists safety. The Police, Fire, and Sheriff Departments have been aware of this planning process and should be involved in implementation. In many cases, citizens (and even sometimes officers) are not fully aware of state and local laws related to bicyclists and pedestrians. Training on this topic can lead to additional education and enforcement programs that promote safety. Training for Rome-Floyd County public safety officers, as well as information and grant resources to support public safety education and enforcement campaigns, are available through local, state, and federal programs and organizations.

## Infrastructure Action Steps

While establishing the policies and programs described, the Rome-Floyd County MPO and its government partners should move forward with the design and construction of priority projects. They should also work to identify funding for long-term, higher-cost projects.

### Identify Funding

Achieving the vision defined within this Plan will require, among other things, a stable and recurring source of funding. Communities across the country that have successfully engaged in pedestrian and bicycle programs have relied on multiple funding sources to achieve their goals. No single source of funding will meet the recommendations identified in this Plan. Instead, stakeholders will need to work cooperatively with municipality, state, and federal partners to generate funds sufficient to implement the Plan.

A stable and recurring source of revenue is needed that can then be used to leverage grant dollars from state, federal, and private sources. The ability of local agencies to generate a source of funding for pedestrian facilities depends on a variety of factors, such as taxing capacity, budgetary resources, voter preferences, and political will. It is very important that these local agencies explore the ability to establish a stable and recurring source of revenue for facilities.

Donations from individuals or companies are another potential source of funding. The BPAC should establish an “Adopt a Trail” program as a mechanism to collect these donations for the development of the greenway trail and sidepath recommendations discussed in the Recommendations chapter.

Federal and state grants should be pursued along with local funds to pay for necessary right-of-way acquisition and project design, construction, and maintenance expenses. Also, the MPO should continually maintain a back-log of designed projects that are “shovel ready” to take advantage of future funding opportunities as they become available.

### Complete Short-Term Priority Projects

By quickly moving forward on priority projects, the Rome-Floyd County MPO will demonstrate its commitment to carrying out this Plan and will better sustain the enthusiasm generated during the public outreach stages of the planning process. Refer to the Recommendations chapter for a list of priority projects.

## Infrastructure Implementation Strategies

This section describes different construction methods for the proposed bicycle and pedestrian facilities outlined in the Recommendations chapter. Note that many types of transportation facility construction and maintenance projects can be used to create new facilities. It is much more cost-effective to provide facilities during roadway construction and re-construction projects than to initiate the improvements later as “retrofit” projects.

To take advantage of upcoming opportunities and to incorporate bicycle and pedestrian facilities into routine transportation and utility projects, the Rome-Floyd County MPO should keep track of GDOT, Floyd County, and City of Rome projects and any other local transportation improvements. While doing this, MPO and government staff should be aware of the different procedures for state and local roads and interstates.

### Rome-Floyd County MPO Transportation Improvement Program

The Rome-Floyd County MPO’s Transportation Improvement Program (TIP) is based on the Rome-Floyd County MPO’s Long Range Transportation Plan, which is now called Long Range Transportation Plan 2040. The TIP is a federally mandated transportation planning document that details transportation improvements prioritized by stakeholders for inclusion in the Work Program over the next ten years. The TIP is updated every year.

### Local Roadway Construction or Reconstruction

Pedestrians and bicyclists should be accommodated any time a new road is constructed or an existing road is reconstructed. In the longer-term, all new roads with moderate to heavy motor vehicle traffic should have sidewalks and safe crossings at intersections. Bicyclists should have dedicated space in the form of a bike lane or buffered facility. However, sidepaths can be an acceptable solution when a road has few driveways and high-speed, high-volume traffic.

Also, case law surrounding the Americans with Disabilities Act (ADA) has found that roadway resurfacing constitutes an alteration, which requires the addition of curb ramps at intersections where they do not yet exist. The Department of Justice and the Federal Highway Administration recently released guidance on the Title II of the ADA requirement to provide curb ramps when streets, roads, or highways are altered through resurfacing. More information is available on the following website:

<http://www.ada.gov/doj-fhwa-ta.htm>.

## Residential and Commercial Development

The construction of sidewalks, trails, and safe crosswalks should be required during development. Construction of facilities that corresponds with site construction is more cost-effective than retrofitting. In commercial development, emphasis should also be focused on safe pedestrian and bicyclist access into, within, and through large parking lots. This ensures the future growth of the pedestrian network and the development of safe communities.

## Repaving

Repaving projects provide a clean slate for revising pavement markings. When a road is repaved, the roadway should be re-stripped to create narrower lanes and provide space for bike lanes and shoulders, where feasible.

## Retrofit Roadways with New Bicycle and Pedestrian Facilities

There may be critical locations in the bicycle and pedestrian network that have safety issues or are essential links to destinations. In these locations, it may be justifiable to add new facilities before scheduling a roadway to be repaved or reconstructed. In some other locations, it may be relatively easy to add a sidewalk or to add extra pavement for bike lanes, but other segments may require removing trees, relocating landscaping or fences, or re-grading ditches. Retrofitting roadways with sidepaths creates similar challenges.

Some roads may require a “road diet” solution in order to accommodate bicycle and pedestrian facilities. Road diets involve reallocating motor vehicle travel lanes for the benefit of increasing roadway safety and efficiency for all users, and in some cases, increasing space for other uses such as parking, on-street bicycle facilities, sidewalks, and/or sidepaths. These are generally recommended only in situations where the vehicular traffic count can be safely and efficiently accommodated with a reduced number of travel lanes. When considering how a road diet might affect road capacity, however, it is important to keep in mind that pedestrian and bicycle facilities may increase roadway capacity by allowing a greater number of total people and vehicles - including pedestrians and bicycles - to move along the roadway in a given time period. Further study may be necessary for recommended road diets to ensure that the needs of all road users are being met.

## **Bridge Construction or Replacement**

Provisions should always be made to include a walking and bicycling facility as a part of vehicular bridges. All new or replacement bridges should accommodate two-way travel for all users. Even though bridge construction and replacement does not occur regularly, it is important to consider these policies for long-term pedestrian planning.

## **Easements**

The Cities of Rome and Cave Spring, along with Floyd County, should explore opportunities to revise existing easements to accommodate public access greenway trail facilities. Similarly, as new easements are acquired in the future, the possibility of public access should be considered. Sewer and utility easements are very commonly used for this purpose, offering cleared and graded corridors that easily accommodate trails. This approach avoids the difficulties associated with acquiring land, and it better utilizes the local government resources.

## Final Draft Public Comment

During the public comment period for the final draft, one comment was received. Below is the comment and a note about how it is addressed in the plan.

From: hdahlmann@berry.edu [mailto:hdahlmann@berry.edu]

Sent: Saturday, February 14, 2015 11:05 AM

To: harrisl@floydcountygga.org

Subject: Comment on current draft of Bicycle, Pedestrian, and Trail Master Plan

Hello,

I just finished reviewing the January draft of the Bicycle, Pedestrian, and Trail Master Plan. It is a beautiful document and very informative. One concern I have is that it is not clear what is going to be done to improve the sidewalk situation (i.e., total lack of sidewalks) in certain areas, especially in West Rome. I live in West Rome and think it is pathetic to have to hike through yards along Garden Lakes Blvd to get to the store or nearest bus stop. It feels like a developing country out here. It is also pathetic that kids who live down the street cannot even safely walk to Garden Lakes Elementary school and get dropped off by car instead.

I would also encourage the committee to consider installing mixed use bike/walkways along the entirety of every bus route in Rome. As it is right now, people have to hike or drive wheelchairs over the lumpy dirt on the side of Veteran's Memorial Highway and other main roads to get to and from the random bus stops. It is sad and very dangerous that in some cases, wheelchair-bound people have to roll down the curb lane of these busy streets since there is no sidewalk or way to get over the curb. I hope the planning commission will expand their focus to include West Rome in the future.

Regards,

Heidi Dahlmann

Submitted By: Heidi Dahlmann

### How the comment is addressed

Regarding sidewalk improvements, the plan identifies areas along major corridors and within a quarter mile of schools and parks as high priority pedestrian areas. The recommended Bicycle and Pedestrian Advisory Committee (BPAC) and government departments and agencies tasked with sidewalk construction should prioritize sidewalk maintenance and construction in these areas.

Regarding access to bus stops, the plan outlines policies for the priority pedestrian areas including closing gaps in the sidewalk network, making sidewalks and crossings universally accessible, providing frequent and safe crossings, convenient bus stop location and providing adequate pedestrian crossing on bridges and underpasses.

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